



East Lake Communities Planning Area Elements

~~Public Review~~Revised Draft - ~~February~~Summer
2026

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EAST LAKE COMMUNITIES PLANNING AREA ELEMENTS

Introduction

This chapter presents the General Plan elements specific to the East Lake Communities Planning Area: Land Use; Circulation; Open Space, Conservation, and Recreation; Environmental Justice; Public Facilities and Services; and Economic Development. The chapter also includes a section on Special Study Areas that ~~offer~~**offers** more detailed policy and design guidance for three subareas in the Planning Area.

This chapter, combined with the countywide elements, ~~make~~**makes** up the General Plan for the East Lake Communities Planning Area. The countywide elements applicable to all areas are Health and Safety, Noise, Geothermal Resources, Agricultural Resources, Water, Housing, and the Aggregate Resources Management Plan.

In recognition of the deep and ongoing connection of the Southeastern Pomo, Eastern Pomo, and Patwin Wintun people native to the East Lake Communities region, this General Plan acknowledges the area as part of the ancestral homelands of these Tribes. The County affirms its commitment to working collaboratively with Tribal governments to preserve and promote Tribal Cultural Resources, sacred sites, and Tribal Ecological Knowledge.

The General Plan incorporates the perspective of tribal communities to ensure that planning decisions reflect the values, history, and rights of the original stewards of this land. Coordination with Tribal governments guides:

- Identification and protection of cultural and sacred landscapes
- Integration of Tribal Ecological Knowledge into land use, conservation, and resource management decisions.
- Consultation processes that respect Tribal sovereignty and the government-to-government relationship between Lake County and federally recognized Tribal governments.
- Inclusion of tribal perspectives in Environmental Justice and Economic Development strategies to support cultural revitalization and community well-being.

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The County recognizes that sustainable planning for the East Lake Communities Planning Area requires honoring the cultural continuity, environmental stewardship, and historical resilience of the Southeastern Pomo, Eastern Pomo, and Patwin Wintun people indigenous to the land.

Location and Setting

The East Lake Communities Planning Area (sometimes referred to as the Planning Area) borders the North Lake Communities Planning Area to the north, Clear Lake to the west, and the Lower Lake Planning Area to the south, with Colusa County forming its eastern boundary. Much of the Planning Area follows the eastern shoreline of Clear Lake, with Mount Konocti on the opposite shore forming the backdrop to the area's iconic views. At nearly 300 square miles, the East Lake Communities Planning Area encompasses 23 percent of the county's land area and is the second-largest Planning Area in terms of land area. While not included in the Planning Area, the incorporated City of Clearlake is ~~situated~~ near the southern portion of the Planning Area along the southeastern shore of Clear Lake; the area within the city limit is excluded from the Planning Area boundary because the City governs the area. Some information is provided in this document that relates to areas within the city limits because it is relevant to the Planning Area and/or provides contextual details, though the goals, policies, and actions of the General Plan only apply to the unincorporated areas of the community. **Figure EL-1** shows the location of the East Lake Communities Planning Area in relation to the other seven Planning Areas and two incorporated cities in Lake County.

The East Lake Communities Planning Area encompasses portions of Clear Lake and Mendocino National Forest. The topography in the Planning Area ranges from chapparal and oak woodland valleys and foothills rising-uprising from the Clear Lake shoreline along the southern and western boundaries of the Planning Area, to more rugged, mountainous terrain extending further north and east. The Clear Lake shoreline is the lowest point in the Planning Area, at approximately 1,330 feet above sea level. The highest point, at 4,804 feet above sea level, is Windy Point, located in the northernmost part of the Planning Area. The terrain in the southeastern part of the Planning Area, bounded by the City of Clearlake and Indian Valley Reservoir to the east and Cache Creek to the south, is characterized as “semi-badlands.” This area is relatively dry and features low rolling hills that are either bare soil and rock, or patched with low, woody, drought-tolerant shrubs (i.e., chaparral).

The East Lake Communities Planning Area has a Mediterranean climate. Summers are dry and warm to hot, with temperatures frequently exceeding 100 degrees Fahrenheit. Winters are cool and wet with some frost and snowfall at higher elevations. The mountainous parts of the Planning Area near the northern and eastern boundaries are the rainiest and most susceptible to annual snowfall.

This map illustrates the planning areas and incorporated cities of Lake County, California. The map is divided into several planning areas, each with a distinct color and boundary. The incorporated cities are shown as solid black areas. The map also includes a legend, a scale bar, and an inset map showing the county's location within California.

Planning Areas:

- North Lake Communities:** Located in the northern part of the county, including Lake Pillsbury Ranch, Witter Springs, Saratoga Valley, Bachelor Valley, Upper Lake, and Blue Lakes.
- East Lake Communities:** Located in the eastern part of the county, including Paradise Valley, Clearlake, Spring Valley, Double Eagle Ranch, Clearlake, and Lower Lake.
- Lower Lake:** Located in the southern part of the county, including Harbin, Hot Springs, Hidden Valley, and Middletown.
- Middletown:** Located in the southern part of the county, including Middletown.
- Cobb Mtn.:** Located in the southern part of the county, including Cobb, Whispering Pines, Anderson Springs, and Pipe Grove.
- Rivieras:** Located in the southern part of the county, including Kelseyville, Kelseyville, and Soda Bay.
- Lakeport:** Located in the western part of the county, including Lakeport, Finley, and Lakeport.

Incorporated Cities:

- Lakeport
- Kelseyville
- Riviera
- Cobb Mtn.
- Lower Lake
- Middletown

Legend:

- Planning Area Boundary (thick black line)
- Incorporated City (solid black area)
- Incorporated City Sphere of Influence (dashed black line)
- Airport (airplane icon)
- Unincorporated Community or Landmark Area (black dot)

Scale: 0 to 20 Miles.

Source: Lake County, 2024; PlaceWorks, 2024.

The Planning Area includes the following communities:

- **Clearlake Oaks**, a census-designated place on the northeast shore of Clear Lake. Many homes in Clearlake Oaks are situated along the Clear Lake Keys, offering direct water access to the lake. Commercial development is situated along Highway 20 and in a town center called “The Plaza” at the western end of the town.
- Clearlake Park, a small lakefront community on the southern base of ‘The Gooseneck’ peninsula northwest of the City of Clearlake.
- **Double Eagle Ranch**, a remote residential subdivision in the eastern portion of the Planning Area, approximately 15 miles northeast of the City of Clearlake.
- **Glenhaven**, a small residential community approximately four miles west of Clearlake Oaks along the shore of Clear Lake.
- **Kono Tayee**, a small community on the shore of Clear Lake in the southwestern part of the Planning Area, just south from the community of Lucerne in the North Lake Communities Planning Area.
- **Paradise Valley**, a small community on the Clear Lake shoreline immediately east of Kono Tayee.
- **Spring Valley**, a small, relatively remote community in the northeastern portion of the Planning Area, southwest of Indian Valley Reservoir.
- Windflower Point, a small lakefront community on the northern tip of ‘The Gooseneck’ peninsula northwest of the City of Clearlake.



Festive bunting on the bridge to Clark's Island in Clearlake Oaks contributes to the Planning Area's small-town charm.

The southern part of the Planning Area borders the City of Clearlake and is bisected by State Route (SR) 53 where it meets SR 20. Clearlake was incorporated in 1980 and is the largest incorporated city in Lake County, encompassing approximately 11 square miles.

Outside of these primarily residential communities, much of the East Lake Communities Planning Area is made up of publicly accessible recreational land, including a large portion of Mendocino National Forest in the northwestern part of the Planning Area- and Berryessa Snow Mountain National Monument stretching across the eastern part of the Planning Area. Agricultural and rural land dominate the central portions of the Planning Area between the Clear Lake shoreline and the mountains. SR 20 is the primary roadway connecting most communities within the Planning Area. SR 20 runs from the western boundary with the North Lake Communities Planning Area along the northern shoreline of Clear Lake until Clearlake Oaks, where it continues eastward to Colusa County. Another major route in the Planning Area is SR 53, which meets SR 20 just east of Clearlake Oaks and runs southward towards the City of Clearlake and to the Lower Lake Planning Area. Circulation in the East Lake Communities Planning Area is highly dependent on highway transportation.

The 2023 American Community Survey (ACS) administered by the U.S. Census estimates that the population in the East Lake Communities Planning Area is approximately 4,490 residents. According to America's Labor Market Information System (ALMIS) Employer Database, in 2024, most residents in the Planning Area work in Clearlake. The largest employer in Clearlake is Adventist Health Clear Lake, a combined hospital and outpatient clinic employing between 350 and 750 people (according to ALMIS's Employer Size Class Capacity categorization). The second-largest employer in the area is Meadow Nursing Center, a skilled nursing care facility also in the City of Clearlake. The Safeway grocery store and Walmart supercenter in Clearlake are also major employers for residents in the Planning Area, as well as major shopping destinations.¹ Economic activity in the Planning Area itself is centered primarily around lodging and recreation opportunities along the Clear Lake shoreline.

The Planning Area's identity is shaped in large part by access to Clear Lake as well as the iconic lake views seen from all over the mountainous parts of the Planning Area. Clear Lake is the largest lake wholly within the state of California and one of the oldest lakes in North America. As is the case for many other parts of Lake County, lake-centered tourism in the late nineteenth and early twentieth centuries catalyzed development in the area. Abundant lakeside resorts formerly lined the eastern shore of Clear Lake. Further east of the lake, visitors could find quieter retreats among natural mineral springs, like the historic Bartlett Springs resort. Although many of these resorts closed after this initial boom, there are still resorts and recreational opportunities found along the northern shore of Clear Lake today, offering popular activities like swimming, watersports, ~~and boating~~, ~~horseback riding~~, and hiking. Revitalizing the resort and tourism economy is a priority for supporting economic development in the Planning Area.

Today, residential communities constitute the dominant land use along the Clear Lake shoreline in the Planning Area. These communities are connected by SR 20 as it winds along the water's edge from Kono Tayee near the northeastern Planning Area boundary to Clearlake Oaks, and then by SR 53 to reach the City of Clearlake at the southern boundary. More remote, rural residential areas like Double Eagle Ranch and Spring Valley are scattered through the mountainous eastern portion of the Planning Area. Residents throughout the Planning Area treasure public community space, like Nylander Park in Clearlake Oaks, and desire greater investment in local retail, safety infrastructure, and public services.

Much of the eastern part of the Planning Area lies within Mendocino National Forest, which comprises over 900,000 acres total and spans across Lake, Colusa, Glenn, and Mendocino Counties. There are multiple access points to the National Forest in the Planning Area. Recreation opportunities include camping, backpacking, off-highway vehicle travel, fishing, hunting, and nature observation. Other popular destinations in the Planning Area include ~~retreat centers~~, the Indian Valley Reservoir, ~~Redbud Park and Trailhead~~, Judge Davis Trailhead, and several wineries.

¹ <https://labormarketinfo.edd.ca.gov/majorer/countymajorer.asp?CountyCode=000033>

History and Planning Context

The Southeastern Pomo, Eastern Pomo, and Patwin Wintun Native American tribes are the historic inhabitants and original stewards of the area. The Eastern Pomo occupied the northern part of the Planning Area near what is now Kono Tayee and in the neighboring North Lake Communities Planning Area. The Southeastern Pomo primarily occupied the area south of there, along the eastern shores of Clear Lake between what is now Kono Tayee and the City of Clearlake. The Patwin Wintun tribes resided in the mountainous parts of the Planning Area east of Clear Lake and south of Stony Creek.

The first European settlers to occupy the area were Spanish-Mexican Captains Salvador Vallejo and Ramon Carrillo, who led an expedition into Clear Lake in 1835 and made claim to the valley for raising livestock around 1840, through land grants offered by the Mexican government. In 1848, California became part of the United States and a new wave of settlers arrived in Lake County beginning in 1854. In 1856, a scientist named John Allen Veatch discovered mineral resources at Borax Lake and established the California Borax Company, which mined borax and some sulfur before ultimately collapsing in 1868. Toward the end of this period, deposits of cinnabar, the mineral form of mercury, were discovered by prospectors in the same area. New owners purchased the defunct California Borax Company in 1873, turned it into a mercury mine, and renamed the company as the Quicksilver Mining Company in 1875. Present-day Clearlake Park and Clearlake Oaks grew as nearby population centers that supported mining operations at Borax Lake. Towards the end of the 1800s, hot spring and lakeside resorts began to proliferate in the East Lake Communities Planning Area. Mineral hot springs in the area were touted for their healing capabilities, drawing visitors seeking relaxation, recreation, and treatment for ailments like rheumatism. Bartlett Springs Resort was named after Bartlett Creek, a tributary of Cache Creek in the northern portion of the Planning Area. The resort, one of the county's largest in the early twentieth century, could accommodate 1,000 guests in the five hotels and 350 cabins on its 160-acre property, and housed a resident physician each summer. In addition to lodging, the resort included a general store, drugstore, bakery, and butcher's shop. Hough Springs Resort, about seven miles east of Bartlett Springs Resort, featured a hotel as well as cottages and tents. By 1910, it could board up to 110 guests. As mineral springs became more popular, more resorts developed in the Planning Area, which in turn led to development and road construction. In the 1930s, the Great Depression significantly impacted the Planning Area's tourism industry, leading to many closures. Other establishments were destroyed in major fires and abandoned.

Residential subdivisions were introduced in the East Lake Communities Planning Area in the 1920s. These included Clearlake Park, formerly Jack’s Landing, and ~~the Clearlake Highlands, which are now part of the City of Clearlake Stubb Subdivision~~, along with the area that is present-day Clearlake Oaks.

Environmental Justice in the General Plan

While each General Plan element addresses specific aspects of county life, these topics often overlap. This is especially true for the topic of environmental justice, which is the ideal state achieved when all people, regardless of race, income, or other factors, are able to live in a safe and healthy environment. The Environmental Justice Element of the General Plan identifies specific locations in the Planning Area, termed Equity Priority Areas, and certain subgroups of the population, termed Equity Priority Populations, experiencing the greatest barriers to achieving environmental justice. ~~the Equity Priority Areas are mapped in Figure EL-12 in the Environmental Justice Element.~~ Because environmental justice touches all aspects of the community, there are policies throughout the General Plan that call for prioritizing the needs of these Equity Priority Areas and Equity Priority Populations to support environmentally just outcomes.

Equity Priority Area is a designation applied to federally recognized tribal lands and certain areas with concentrations of low-income households, identified as being at risk of suffering disproportionate impacts of economic, health, and environmental burdens. Equity Priority Areas are mapped in **Figure EL-12**.

Equity Priority Populations are community members with characteristics often associated with being vulnerable to environmental justice, health, and safety challenges. These Equity Priority Populations include older adults, individuals with disabilities, farmworkers and other outdoor workers, non-English speakers, and low-income individuals and households.

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Land Use

Land use patterns and related policies play an important role in defining local character. The scale, location, and type of development influence the sense of community or feeling of being in nature. Street trees and local art enhance the sense of place, while vacant lots with overgrown vegetation and litter suggest neglect. The location and design of lighting can determine whether stars are visible.

This Land Use Element fosters continued conservation of agricultural lands and open space while supporting cohesive communities. It shapes the future physical development of the East Lake Communities Planning Area and works to conserve and enhance livability and quality of life for residents. The element also supports the protection of cultural and sacred sites, traditional gathering areas, and landscapes of ongoing tribal significance as well as the incorporation of Tribal Ecological Knowledge (TEK) in land management, conservation, and fire recovery practices to enhance environmental resilience and uphold the cultural heritage of the area. This element begins by presenting the General Plan Land Use Map and designations, which establish regulations and standards for development. The map is supported by goals, policies, and actions that follow, upholding the principles on which the map was developed, including balancing growth and conservation and preserving community character in the Planning Area.



Land use designations help to ensure compatibility among adjacent uses.

Land Use Designations and Map

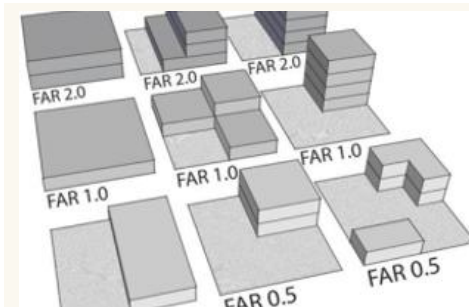
State planning law requires the Land Use Element to designate the general distribution, location, and extent of the various land uses covered by this General Plan. The General Plan Land Use Map is presented in **Figures EL-2** (Countywide Land Use Map) and **EL-3** (East Lake Communities Area Land Use Map). The following sections describe each land use designation shown in the maps, including the standards for allowed density of residential uses and intensity of nonresidential use. The density and intensity of designations are summarized in **Table EL-1**. [Finally, Appendix E includes a comparison of land use designation acreages between this Plan and the previously adopted General Plan.](#)

Standards for residential densities are expressed in dwelling units (DU) per gross acre, while development intensities for nonresidential uses are expressed as maximum floor-area ratios (FAR). Gross acreage includes all land (including streets and rights-of-way) designated for a particular residential use. Expressed as a decimal, FAR is the ratio of gross building square footage to gross land area. When a building's square

footage is equivalent to the area of its lot, the FAR is 1.0. For example, on a lot of 10,000 square feet, a FAR of 1.0 would allow a building of 10,000 square feet. On the same lot, a FAR of 0.5 would allow 5,000 square feet.

Substandard Older Subdivisions

Prior to the preparation of General Plan 2050, throughout Lake County, including in the East Lake Communities Planning Area, there were approximately 10,000 undeveloped older subdivided properties with urban or suburban densities, known as “paper subdivisions,” primarily designated Rural Residential by the General Plan. These properties are substandard in relationship to existing zoning and subdivision regulations with design, size, or physical improvements that do not meet County standards. These parcels are characterized by steep slopes and lack of adequate public services and facilities such as streets, fire protection, sewer, or water services. As a part of General Plan 2050, these parcels were redesignated to Public Facilities in recognition that they are not developable and better suited for future open space, trail access, or watershed protection. As part of the process to implement the General Plan, these parcels will need to be rezoned to account for the new land use designation.



This illustration shows how various building configurations represent different FARs on similarly sized lots. This graphic is intended only to show the relative differences in FAR and does not represent an intensity standard for this General Plan.

Community Growth Boundaries

Figures EL-2 and EL-3 also show community growth boundaries (CGBs). CGBs are regulatory tools that support both development and conservation goals by preserving open space between communities and maintaining community separation and identity. These boundaries define areas where residential and commercial development may occur by dividing land to be developed at higher densities (1 or more DU/acre) from land to be protected for conservation or developed at lower densities (1 DU/5 or more acres). Land beyond the boundary of the CGB is preserved for uses such as agriculture, forestry, open space, greenbelts, and conservation. The East Lake Communities Planning Area includes one CGB for Clearlake Oaks, situated a few miles northwest of Clearlake. The area within the CGB is largely residential, with the greatest concentration of residential development bordering Clear Lake to the south. The area also includes commercial (resort, community, and service), public facilities, and resource conservation lands.

Agriculture

A main function of this land use designation is to protect the county's valuable agricultural resources and prevent development that would preclude its future use in agriculture. These lands are actively or potentially engaged in crop production, including horticulture, tree crops, row and field crops, and related activities. Wineries and the processing of local agricultural products, such as pears, cannabis, and

walnuts, are encouraged within this designation. Other agriculture-supporting uses, such as microbreweries, distilleries, agriculture equipment sales, youth agriculture, and wilderness education activities, are also allowed. These lands also provide important groundwater recharge functions. As watershed lands, they collect precipitation and filter water to improve water quality. They are generally supportive of the management of the natural infrastructure of the watersheds. While there are some existing lands with this designation inside CGBs, future applications of the designation should be limited to areas outside CGBs. [Agricultural operations are protected from nuisance complaints by the County's Right to Farm Ordinance, located in Chapter 3 \(Agriculture\), Article IV, of the County Code of Ordinances. The Right to Farm Ordinance is discussed in the Agriculture Element.](#)

Density/Intensity: 1 DU/40 acres

Figure EL-2 Countywide Land Use Map

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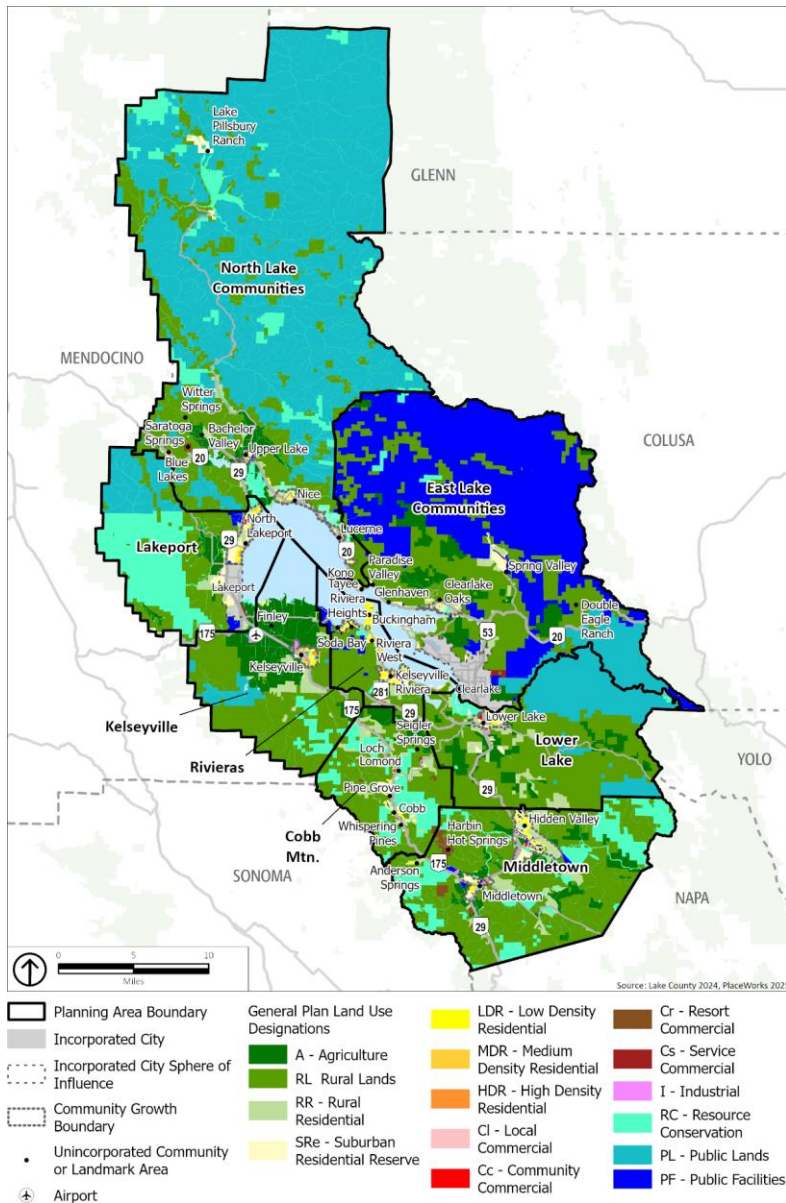
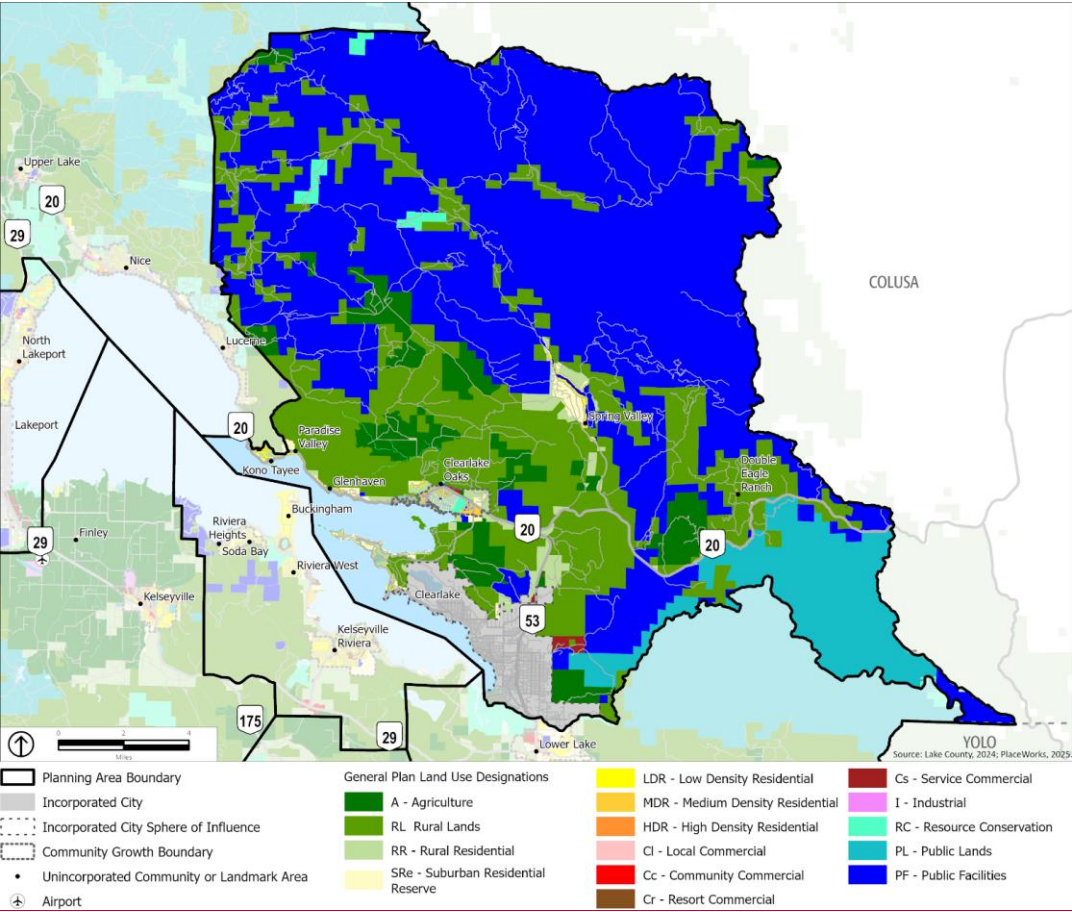


Figure EL-3 East Lake Communities Area Land Use Map

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Rural Lands

The purpose of this land use designation is to allow rural development in areas that are primarily in their natural state, although agricultural production, including farming, cannabis cultivation, and vineyards, can occur on these lands. The category is appropriate for areas that are remote, or characterized by steep topography, fire hazards, and limited access. Typical uses permitted by right include, but are not limited to, animal raising, crop production, single-family residences, game preserves, and fisheries. Other typical uses permitted conditionally include, but are not limited to, recreational facilities, manufacturing and processing operations, mining, and airfields. These lands also provide important groundwater recharge functions. As watershed lands, they collect precipitation and filter water to improve water quality. These uses generally support the natural infrastructure of the watersheds and are outside of CGBs.

Density/Intensity: 0.4 FAR; 1 DU/20-45+ acres

Average slope describes the “rise over run,” or the degree of vertical increase divided by the degree of horizontal increase of a sloped area, and determines the extent of allowable development. The base density of 1 DU/20 acres shall be reduced when the average slope of the property increases, as follows:

- 1 DU/30-acre maximum density if average slope is 20 percent or higher, but less than 30 percent
- 1 DU/40-acre maximum density if average slope is 30 percent or higher, but less than 35 percent
- 1 DU/45+-acre maximum density if average slope is 35 percent or higher

The maximum density in the Rural Lands designation may be reduced based on factors related to wildland fire/fuel loading, landslide hazards, and distance to the nearest fire station, as enumerated in the Zoning Ordinance.

Rural Residential

This land use designation is designed to provide single-family residential development in a semi-rural setting. Large-lot residential development with agricultural activities is appropriate. These areas are intended to act as a buffer between urban residential development and agricultural uses. Building intensity should be greater where public services, such as major roads, community water systems, or public sewer are available, although most of the lands designated for this land use rely on wells and septic systems. Typical uses permitted by right include single-family residences; crop production (including cannabis); raising poultry, rabbits, and other small animals for domestic use; raising bovine animals, horses, sheep, and goats for domestic use; and sale of crops produced on the premises. Typical uses permitted conditionally include agricultural-related services and recreational facilities. These lands also provide important groundwater recharge functions. As watershed lands, they collect precipitation and filter water to improve water quality. They are generally supportive of the management of the natural infrastructure of the watersheds. This designation is primarily outside of CGBs, but some areas will be appropriate inside these boundaries as well.

Density/Intensity: 1 DU/5-10+ acres

- 1 DU/5 acres if average slope is less than 30 percent and outside an area identified with Class VII-X Landslide Susceptibility
- 1 DU/10+ acres if average slope is 30 percent or greater, or in an area identified with Class VII-X Landslide Susceptibility

Suburban Residential Reserve

This land use designation serves as a transitional designation between rural residential and urban residential uses, but due to soil and slope characteristics, lots generally larger than one-acre minimum are necessary. Larger lot residential development with small-scale, incidental agriculture is appropriate.

Where found in mountain areas, this designation serves to limit densities until the establishment of appropriate infrastructure to support higher densities. However, exceptions have been made for existing subdivisions outside of CGBs, and additional land division may be appropriate on parcels already designated Suburban Residential Reserve when found to be consistent with the existing development pattern already established in the vicinity. ~~Expansion of this land use designation outside of CGBs may be considered to implement Action EL-LU-5.d for appropriate mixed-use resort or business/industrial park developments.~~ This designation has the potential to result in conflict when located on the fringe of CGBs, adjacent to agricultural uses, unless cluster policies are employed and agricultural buffers are provided. This designation is mostly outside of CGBs, but some parcels with this designation are inside CGBs.

Density/Intensity: 1 DU/1-5 acres

- 1 DU/acre if the area is not identified as a primary recharge area and the area has less than 30 percent average slopes
- 1 DU/2 acres if the area is identified as a primary recharge area and the area has less than 30 percent average slopes
- 1 DU/3 acres if the area has 30 percent or higher average slopes, but is not in an area identified with Class VII-X Landslide Susceptibility in the Health and Safety Element
- 1 DU/5 acres if the area has 30 percent or higher average slopes and is in an area identified with Class VII-X Landslide Susceptibility in the Health and Safety Element

Low-Density Residential

This land use designation is designed to establish areas suitable for single-family residences at relatively low densities. Typical uses permitted by right include single-family homes. This designation is appropriate where the traditional neighborhood character of single-family units prevail, and where the level of services, including roads, shopping, and recreation, does not justify higher densities. While there are some existing lands with this designation outside of CGBs, future applications of the designation should be limited to areas within CGBs.

Density/Intensity: 1-~~6~~7 DU/acre

In hazard-prone areas, such as areas with slopes exceeding 20 percent, flood hazard zones, High and Very High Fire Hazard Severity Zones, Class VII-X Landslide Susceptibility areas, and Alquist-Priolo Earthquake Fault Zones, reduced residential densities below the minimum density may be allowed.

Medium-Density Residential

This land use designation is designed to provide areas for residential developments such as duplexes, triplexes, and manufactured homes. This designation is appropriate only where the level of development is compatible with the overall character of the surrounding neighborhood and where adequate public facilities, such as potable water and sewer systems, roadways, and other public services are available. While there are some existing lands with this designation outside of CGBs, future applications of the designation should be limited to areas within CGBs.

Density/Intensity: ~~7~~6-9 DU/acre

In hazard-prone areas, such as areas with slopes exceeding 20 percent, flood hazard zones, High and Very High Fire Hazard Severity Zones, Class VII-X Landslide Susceptibility areas, and Alquist-Priolo Earthquake Fault Zones, reduced residential densities below the minimum density may be allowed.

High-Density Residential

This land use designation is designed to provide for areas of multifamily residential uses that include a wide range of living accommodations, including duplexes, townhouses, and apartments. This designation is appropriate in community areas where a full range of urban services are available, including public water and sewer, adequate circulation facilities, and adequate public services, and where the level of development is compatible with the surrounding neighborhood. This designation is only within CGBs.

Density/Intensity: 10-22 DU/acre

In hazard-prone areas, such as areas with slopes exceeding 20 percent, flood hazard zones, High and Very High Fire Hazard Severity Zones, Class VII-X Landslide Susceptibility areas, and Alquist-Priolo Earthquake Fault Zones, reduced residential densities below the minimum density may be allowed.

Local Commercial

The purpose of this land use designation is to establish areas for small, localized retail, recreational, and service businesses that provide goods and merchandise to the immediate surrounding residents. Convenience facilities may include eating and drinking establishments; food and beverage retail sales; limited personal, medical, professional, and repair services; and other retail sales. Such facilities may range from a single use to a cluster of uses. Residential uses are also allowed in this designation, either as the primary or secondary use. This designation is primarily within CGBs; it may be allowed outside of a CGB to expand an existing commercial development that is already outside of the boundary.

Density/Intensity: 0.5 FAR; ~~where residential uses are allowed with a combining district, the allowed density is 1-9 DU/acre~~ ~~6-9 DU/acre~~.

Community Commercial

The purpose of this land use designation is to provide a full range of commercial retail and service commercial establishments serving multiple neighborhoods or the entire community. These districts should include or enable a variety of convenience and shopping opportunities. Typical uses permitted include gasoline service stations, hardware stores, eating and drinking establishments, food and beverage sales, public buildings, general merchandise stores, professional offices, and finance offices. Multifamily residential uses are permitted as secondary uses on upper floors of multistory buildings. Commercial accessory residential uses are also allowed. This designation is primarily within CGBs, and may be allowed outside of a CGB to expand existing commercial development that is already outside of the boundary.

Density/Intensity: 1.2 FAR; when development includes mixed uses, the maximum FAR is 2.0 and the maximum residential density is ~~1-22 DU/acre~~ ~~10-22 DU/acre~~.

Resort Commercial

This land use designation provides for a mix of commercial uses oriented toward tourists and other visitors to the county, including agriculturally based tourism. Typical uses permitted include recreation activities (e.g., golf courses); dining; entertainment services; destination resorts; various types of lodging facilities such as, but not limited to, hotels, motels, retreats, fractional ownership lodging units and time-share units; wineries; spas; and on-site residential uses if secondary and subordinate to commercial resort uses. This designation is both inside and outside of CGBs.

Density/Intensity: 1.0 FAR

Service Commercial

The purpose of this land use designation is to provide areas suitable for heavier commercial uses in developed areas. Typical uses permitted with appropriate buffer distances include automotive-related or heavy equipment sales and services, building maintenance services, construction sales and services, and warehousing. This designation is both inside and outside of CGBs.

Density/Intensity: 1.2 FAR

Industrial

This land use designation provides for a range of manufacturing, the processing of natural resources, research facilities and high-tech campuses, and heavy commercial activities. The intent is to encourage sound industrial/heavy commercial development by designating appropriate areas for such uses, including geothermal service yards, large construction/contractor yards, warehouses, asphalt batch

plants, mills, lumber yards, boat building, welding, and fabrication shops. This designation is both inside and outside of CGBs.

Density/Intensity: 1.5 FAR

Resource Conservation

The purpose of this land use designation is to assure the maintenance or sustained generation of natural resources in the county. The highest priority for these lands is to provide for the management of the county's natural resources. This management should include, but is not limited to, functioning as watershed lands, which collect precipitation and filter water to improve water quality. In addition, these lands provide important groundwater recharge capability, which is critical to the maintenance of the natural ecosystem and to providing a sustainable groundwater supply for the county. This category includes significant plant or animal habitats; forest lands in Timberland Preserve Zones; agricultural lands within the Williamson Act; grazing lands; important parts of watersheds, such as waterways and wetlands; outdoor parks and recreation areas; mineral deposits and mining areas that require special attention because of hazardous or special conditions; publicly owned land (e.g., U.S. Forest Service, Bureau of Land Management land, State lands, and County lands); and open space activities. Uses allowed in this designation are those related to resource utilization and resource conservation activities. Resource utilization operations and facilities require a conditional use permit. This designation is both inside and outside of CGBs.

Density/Intensity: Density: 0. Intensity: Not Applicable (determined on a case-by-case basis)

Public Facilities

This land use designation establishes areas for services and facilities that are necessary to maintain the welfare of the community and the natural environment. Typical uses include parks, public marinas, schools, civic centers, hospitals, liquid and solid waste disposal and recycling sites, cemeteries, and public utility facilities, including water and wastewater treatment facilities. This designation does not preclude future facilities from being located outside areas designated Public Facilities. This designation is both inside and outside of CGBs.

The John T. Klaus Trust bequeathed approximately 584 acres of land in unincorporated Lake County to the County of Lake. The Trust stipulates that the 584-acre site is to be used solely as a public park, with the requirement to include one-third of the Project site for a refuge for wildlife, as well as an area of the Project site set aside for youth sporting and athletic activities.

Density/Intensity: Density: 0. Intensity: Not Applicable (determined on a case-by-case basis)

Public Lands

This land use designation identifies lands owned or managed by federal, State, or local public agencies for recreation, education, resource management, public safety, utilities, or other governmental purposes. These lands are generally expected to remain in public ownership, although compatible ancillary uses may occur.

Density/Intensity: Not Applicable (determined on a case-by-case basis)

Table EL-1 Land Use Designations

Land Use Designation	Label	Residential Density	Nonresidential Intensity	Located Inside/Outside Community Growth Boundary
Agriculture	A	1 DU/40 acres	N/A	Outside ¹
Rural Lands	RL	1 DU/20-45+ acres	0.4 FAR	Outside
Rural Residential	RR	1 DU/5-10+ acres	N/A	Inside/Outside ²
Suburban Residential Reserve	SR	1 DU/1-5 acres	N/A	Inside/Outside ³
Low-Density Residential	LDR	1- 7 6 DU/acre	N/A	Inside ⁴
Medium-Density Residential	MDR	6 7-9 DU/acre	N/A	Inside ⁴
High-Density Residential	HDR	10-22 DU/acre	N/A	Inside
Local Commercial	CL	6 1-9 DU/acre ⁵	0.5 FAR	Inside/Outside ³
Community Commercial	CC	1 0 -22 DU/acre	2.0 FAR	Inside/Outside ³
Resort Commercial	CR	--	1.0 FAR	Inside/Outside
Service Commercial	CS	--	1.2 FAR	Inside/Outside
Industrial	I	--	1.5 FAR	Inside/Outside
Resource Conservation	RC	0	N/A	Inside/Outside
Public Facilities	PF	0	N/A	Inside/Outside
Public Lands	PL	N/A	N/A	Inside/Outside

¹ Agriculture is primarily outside CGBs; any future applications of the designations should be limited to areas outside CGBs.

² Rural Residential is primarily outside of CGBs, but some areas will be appropriate inside these boundaries. Refer to the General Plan Land Use designation description for more information.

³ Suburban Residential Reserve, Local Commercial, and Community Commercial are allowed outside of the CGB in limited instances. Refer to the General Plan Land Use designation description for more information.

⁴ Low-Density Residential and Medium-Density Residential are primarily inside CGBs; any future applications of the designations should be limited to areas within CGBs.

⁵ ~~Where residential uses are allowed with a combining district:~~

Goals, Policies, and Actions

Goal EL-LU-1

Balanced and orderly growth and conservation that preserve and promote a high quality of life, safe community, conservation of resources, natural beauty, and the recognition and protection of the cultural and historical heritage of indigenous people.

Policies

EL-LU-1.1

Preserve natural open space resources by concentrating development in existing communities, clustering development, maintaining large lot sizes in agricultural areas, avoiding conversion of lands currently used for agricultural production, and limiting development in areas constrained by natural hazards.

EL-LU-1.2

Encourage clustering of allowable densities to reduce development footprints; protect sensitive habitats, scenic resources, natural features, and open spaces; and avoid hazardous areas.

EL-LU-1.3

Require proposed developments within designated Community Growth Boundaries served by special service districts to demonstrate capacity to sustain growth as a condition of approval.

EL-LU-1.4

Review development proposals for adequate parking while encouraging multi-modal travel options.

EL-LU-1.5

Maintain distinct urban edges for all unincorporated communities, while creating a gradual transition between urban uses and open space.

EL-LU-1.6

Ensure future amendments to the General Plan Land Use map only allow development where it has access to adequate water supply, wastewater treatment, and public roadways; avoids steep terrain, sensitive natural resources, and natural hazards; and provides appropriate buffers with agricultural land and other incompatible uses.

Some of the topics addressed under this goal are also covered in other elements, including:

- Open space and resource conservation—addressed in the Open Space, Conservation, and Recreation Element
- Water and wastewater services—addressed in the Public Facilities and Services Element
- Natural hazards—addressed in the countywide Health and Safety Element

~~EL-LU-1.5~~**EL-LU-1.7**

Limit urban development to the areas within designated Community Growth Boundaries, as shown in **Figures EL-2 and EL-3**.

EL-LU-1.8

Encourage residential development at densities higher than one dwelling unit per 3 acres to be located in areas where infrastructure is available, or can be extended, and applicable source capacity is adequate.

~~EL-LU-1.6~~**EL-LU-1.9**

Consider development intensity in areas outside of Community Growth Boundaries according to the area's ability to support water and wastewater needs, access, slope characteristics, protection of sensitive natural resources, and the site's susceptibility to natural hazards.

~~EL-LU-1.7~~**EL-LU-1.10**

Prohibit uses not related to timber production on lands designated Resource Conservation in Timberland Preserve Zones unless the use:

- Is consistent with the Aggregate Resources Management Plan for mining operations;
- Is consistent with the Geothermal Resources Element for geothermal activities;
- Supports a biomass utilization business that helps reduce threat of wildfire from dead and dying trees;
- Preserves unique natural landmarks;
- Is compatible with sustainable forest practices and uses;
- Is managed to avoid interfering with forest practices;
- Is managed to minimize impacts on viewsheds;
- Minimizes forest site productivity losses; and Meets General Plan and Zoning Ordinance standards relating to the availability of fire protection, water supply, water quality, groundwater recharge and extraction, watershed management or restoration programs, and waste disposal.

~~EL-LU-1.8~~**EL-LU-1.11**

Consult with Tribal governments on discretionary development projects to protect and preserve areas with known Tribal Cultural Resources.

~~EL-LU-1.9~~**EL-LU-1.12**

Integrate Tribal Ecological Knowledge from Tribes into resource conservation strategies, including watershed management, forest management, and habitat restoration.

~~Develop educational materials regarding Traditional Ecological Knowledge practices.~~

Actions

EL-LU-1.a

Amend the Zoning Ordinance to include a provision that would exclude land lakeward of 7.79 Rumsey from being used in the calculation of lot coverage.

The **Rumsey Gauge** is a measurement system used to determine the level of Clear Lake, with “Zero Rumsey” representing the natural low water level and a “full lake” defined as 7.56 feet above that. The gauge is on a pier at the Lake County Vector Control District.

EL-LU-1.b

Work with Tribal governments to explore opportunities to integrate traditional stewardship practices and Tribal Ecological Knowledge (TEK) into development reviews, land management, conservation, restoration, recreation planning, and facility planning, siting, and operations.

EL-LU-1.c

Work with Tribal Governments to promote Tribal co-management of public lands.

EL-LU-1.d

Develop educational materials regarding Tribal Ecological Knowledge practices.

Goal EL-LU-2

Development patterns that support a sustainable lifestyle.

Policies

EL-LU-2.1

Promote and practice the principles of smart growth.

Transportation-related topics discussed under this goal, including transit access, active transportation, and vehicle miles traveled, are also addressed in the Circulation Element.

EL-LU-2.2

Encourage infill development.

EL-LU-2.3

Encourage urban development that locates origins and destinations in close proximity to reduce vehicle trips and enhance visual and pedestrian access.

Smart growth supports choice and opportunity by promoting efficient and sustainable land development, incorporates redevelopment patterns that optimize prior infrastructure investments, and consumes less land that is otherwise available for agriculture, open space, natural systems, and rural lifestyles.

EL-LU-2.4

Encourage potential transit destinations, including employment centers, schools, personal services, administrative and professional offices, and social/recreational centers, to be clustered within a convenient walking distance of one another and to a transit stop.

- American Planning Association, 2012

EL-LU-2.5

Support the development of land uses with increased densities and mixed uses to reduce vehicle miles traveled and promote walking, biking, and transit use.

Actions

EL-LU-2.a

Develop incentives to promote infill development.

EL-LU-2.b

~~Amend the Zoning Ordinance to establish a smart growth overlay zone that encourages higher density housing, transit-oriented development, and mixed-use development and provides guidelines for development and redevelopment in these zones.~~

EL-LU-2.cEL-LU-2.b

Work collaboratively with Tribal governments to develop educational programs highlighting tribal ecological practices, such as controlled burns and habitat stewardship, to inform sustainable development policies.

Goal EL-LU-3

Cohesive and vibrant communities that preserve and celebrate their historic character and design heritage, and tribal cultural identity.

Policies

EL-LU-3.1

Recognize the unique character and assets of each community in the Planning Area and support efforts to strengthen and revitalize all communities.

EL-LU-3.2

Support efforts to enhance gateways and transitional zones between communities in the Planning Area to make each community more distinct and inviting for residents and visitors.

EL-LU-3.3

Ensure future development within Community Growth Boundaries is planned at a walkable, neighborhood scale with a range of uses in close proximity and a safe pedestrian environment.

EL-LU-3.4

~~Collaborate with the Municipal Advisory Council to identify preferred locations for infill, mixed use, public gathering spaces, pedestrian improvements, and tourism related uses.~~

EL-LU-3.4EL-LU-3.5

Promote the creation of community focal points and gathering places in Glenhaven, Kono Tayee, Paradise Valley, and Spring Valley.

~~EL-LU-3.5~~EL-LU-3.6

Encourage and support the arts in Clearlake Oaks and along the shoreline with music and art installations, public art, and programming throughout the community, prioritizing areas that lack them.

~~EL-LU-3.6~~EL-LU-3.7

Encourage and support community and regional events (e.g., outdoor concerts, art shows, farmers' markets, races, and festivals) and recreational activities to bolster community pride and identification, including by engaging in marketing efforts.

~~EL-LU-3.7~~EL-LU-3.8

Encourage preservation of open areas within Community Growth Boundaries to retain a rural character and promote low intensities of development in areas separating communities to retain distinct communities.

~~EL-LU-3.8~~EL-LU-3.9

Review commercial ~~and industrial~~ development to ensure compatibility with the East Lake Area Commercial ~~and Industrial~~ Design Guidelines to be prepared in accordance with **Action EL-LU-3.e**, consistent with Article 54, Design Review Permit, of the Zoning Ordinance.

~~EL-LU-3.9~~EL-LU-3.10

Maintain the character of established neighborhoods by preserving buildings and areas with special and recognized historic, architectural, or aesthetic value. Ensure the design of new development and remodels of existing structures are compatible with the character of surrounding and adjacent buildings, public spaces, and cultural and historic resources and require new development to complement architecturally and historically significant buildings to protect the overall historic integrity of the area.

~~EL-LU-3.10~~EL-LU-3.11

Encourage the preservation and maintenance of existing native trees and the incorporation of native, fire-resistant, and drought-tolerant plants, including culturally significant species traditionally used by local Tribes, in public spaces, street landscaping, and in new public and private developments.

~~EL-LU-3.11~~EL-LU-3.12

Continue to fund code enforcement programs and periodically evaluate enforcement priorities based on community needs ~~and input, including from the Municipal Advisory Council~~.

~~EL-LU-3.12~~EL-LU-3.13

Encourage and support efforts to promote a greater public awareness of the county's architectural, urban design, Tribal, and cultural heritage.

Some of the topics addressed under this goal are also covered in other elements or sections, including:

- Historic preservation and Tribal Cultural Resources—addressed in the Open Space, Conservation, and Recreation Element
- Community design—addressed in the Special Study Areas and Design Guidelines section

~~EL-LU-3.13~~EL-LU-3.14

Recognize and incorporate tribal heritage in public art, interpretive signage, community events, and wayfinding throughout the East Lake Communities Planning Area.

EL-LU-3.15

Encourage community and regional events that comply with parking, traffic, sanitation, and emergency regulations.

Actions

EL-LU-3.a

Coordinate with local non-profit and other nongovernmental organizations to install or maintain cohesive public art and gateway, wayfinding, and interpretive signage in the communities along State Route 20 to support the area's sense of place; foster an understanding and appreciation for local history and culture, including tribal culture; promote inclusivity; and improve the visual quality of each community.

EL-LU-3.b

Investigate a 1 percent assessment on development permits or other types of permits/assessments to fund art installations supported in Action EL-LU-3.a.

~~EL-LU-3.b~~EL-LU-3.c

Amend the Zoning Ordinance to eliminate redundancies in the design review process between the General Plan and Zoning Ordinance. This includes eliminating Article 53, Design Review Combining District, instead relying on **Policy EL-LU-3.89** to ensure compatibility with the East Lake Communities Area Commercial ~~and Industrial~~ Design Guidelines, and revisions to Article 54, Design Review Permit, as needed.

EL-LU-3.c

~~Update the Special Study Areas for the East Clearlake Oaks Commercial District and "The Plaza," working in collaboration with the community to reflect current values and priorities.~~

EL-LU-3.d

Update the Clearlake Oaks Design Guidelines, working in collaboration with the community to reflect current values and priorities, and include a map to clearly identify the subject area.

EL-LU-3.e

Prepare Commercial ~~and Industrial~~ Design Guidelines to support development throughout the Planning Area that is thoughtfully designed and compatible with the existing environment.

EL-LU-3.f

Conduct a public engagement process to explore interest in preparing Special Study Areas to provide detailed policy and/or design guidance addressing unique needs and/or interests of the Double Eagle, High Valley, and/or Spring Valley areas.

EL-LU-3.g

Develop and implement a tree preservation ordinance.

EL-LU-3.gEL-LU-3.h

Work with local preservation groups and community property owners to improve building facades and exteriors, consistent with the historic and visual character of the surrounding area.

EL-LU-3.hEL-LU-3.i

Amend the Zoning Ordinance to allow adaptive reuse of historic resources that preserves the integrity of the resource while allowing for its ongoing use.

EL-LU-3.iEL-LU-3.j

Prepare and publish an inventory of underutilized land and encourage adaptive reuse of these properties.

EL-LU-3.jEL-LU-3.k

Collaborate with Tribal governments to create a heritage trail, interpretive exhibits, or other programming that celebrates tribal history and presence.

Goal EL-LU-4

Coordinated and effective planning over the life of this General Plan.

Policies

EL-LU-4.1

Ensure that permitting procedures and regulations are consistent, timely, and cost-effective.

EL-LU-4.2

Coordinate growth and development with surrounding jurisdictions, the Local Agency Formation Commission (~~LAFGC~~LAFCO), and other agencies, as appropriate, to maintain open space between communities and promote common goals.

EL-LU-4.3

Within the City of Clear Lake Sphere of Influence, refer any development and/or entitlements to the City for comment regarding possible annexation and development within the city.

EL-LU-4.4

Prior to development in the City of Clear Lake Sphere of Influence, work with the City to develop and jointly adopt a plan for territory within LAFCO's ~~adopted~~the adopted the Sphere of Influence and consider consistent joint planning and development standards between the City and County for the purpose of promoting City/County cooperation and orderly development.

EL-LU-4.3EL-LU-4.5

Require that the sizes and shapes of new parcels will not compromise future land divisions.

~~EL-LU-4.4~~EL-LU-4.6

The General Plan Update Environmental Impact Report (EIR) assumes the following maximum development projections in the unincorporated county for the year 2050:

- 3,800 new dwelling units
- 986,700 square feet of new commercial and office space
- 393,000 square feet of new industrial space

If new development approved within the unincorporated county reaches the maximum number of residential units and commercial/office and industrial square feet projected in the General Plan EIR, require that the environmental review that is conducted for any subsequent development project address growth impacts that would occur from development exceeding the General Plan EIR's projections.

EL-LU-4.7

Work with the Municipal Advisory Council, business organizations, and community to update this Planning Area Element in a timely manner.

EL-LU-4.8

Promote co-management of public lands with Tribal governments.

~~EL-LU-4.5~~EL-LU-4.9

Conduct early and ongoing engagement with Tribal governments in planning and permitting processes to ensure development aligns with tribal cultural, environmental, and historical interests.

Actions

EL-LU-4.a

Encourage potential developers to attend pre-development meetings and work closely with appropriate public agencies and municipal advisory councils to consider whether the potential development projects will meet the County's goals and requirements and have adequate infrastructure.

EL-LU-4.b

Track growth to ensure it does not exceed the development projections analyzed in the General Plan EIR and described in **Policy EL-LU-4.46** without subsequent environmental review.

EL-LU-4.c

Update the Planning Area Elements no less than every five to eight years.

~~EL-LU-4.c~~EL-LU-4.d

Update the Zoning Ordinance to revise the development standards, where needed, to ensure consistency with the density and intensity standards established in this General Plan, including for the Suburban Residential Reserve and Rural Residential Districts.

~~EL-LU-4.d~~**EL-LU-4.e**

Update the Zoning Map to implement the land use map established in this General Plan, including to rezone paper subdivisions to the Open Space district.

~~EL-LU-4.e~~**EL-LU-4.f**

Consistent with State law, continue to conduct tribal cultural impact assessments in coordination with local Tribal governments in discretionary permitting and General Plan amendment processes.

Goal EL-LU-5

A range of residential areas that accommodate the housing needs of all income groups with quality living environments.

Policies

EL-LU-5.1

Plan for and encourage a variety of housing types with a full range of affordability.

EL-LU-5.2

Develop housing in a manner that complements the small-town community identity and incorporates quality design, consistent with the rural character of the Planning Area.

Community housing needs are addressed comprehensively in the Housing Element. Additionally, the Housing Action and Implementation Plan serves as a manual with more detailed strategies to address current and forecasted housing need throughout Lake County.

EL-LU-5.3

Encourage development of ‘workforce housing’ that meets the needs of the local workforce and is affordable to individuals and families earning between 60 percent and 120 percent of the area median income.

EL-LU-5.4

Encourage medium-density residential development that is compatible with the neighborhood character within Community Growth Boundaries, is close to services, and where adequate infrastructure is available or can be made available.

EL-LU-5.5

Encourage high-density residential development (e.g., apartments and townhouses) within Community Growth Boundaries along collector roadways and transit routes and within walking distance of schools, libraries, parks, shopping, and entertainment areas.

EL-LU-5.6

Promote development of affordable, **high density** housing near public transit and essential services whenever possible.

EL-LU-5.7

Encourage the development of housing for seniors and other Equity Priority Populations.

EL-LU-5.8

Continue to allow density bonuses for residential development projects that include affordable or qualifying senior housing or incorporate energy-efficient techniques exceeding State standards into the project design, as specified in the Zoning Ordinance.

EL-LU-5.9

Protect residential neighborhoods from incompatible uses and activities that adversely affect public health and safety, like ~~industrial~~ uses that produce hazardous waste.

EL-LU-5.10

Encourage innovative, non-traditional housing designs and layouts in response to evolving housing trends and needs.

EL-LU-5.11

Encourage the replacement of older substandard mobile homes with newer manufactured or stick-built homes.

Actions**EL-LU-5.a**

Continue to support construction of affordable housing, including by pursuing State and federal funding for affordable housing development.

EL-LU-5.b

Develop incentives for affordable housing, in addition to density bonuses, to further encourage its development.

EL-LU-5.c

Update the density bonus allowances in Article 27 of the Zoning Ordinance to allow a density bonus for residential development projects that include affordable housing that is consistent with State law, which offers a density bonus of up to 50 percent.

EL-LU-5.d

Update the density bonus allowances in Article 27 of the Zoning Ordinance to allow up to a 20 percent density bonus for mixed-use residential development projects within a Community Growth Boundary that include an employment-inducing industrial or business park development component.

EL-LU-5.e

Consider updating the density bonus process to minimize government obstacles to housing development, consistent with State law.

EL-LU-5.f

Continue to promote the development of affordable, senior, and energy-efficient housing by informing developers of the benefits of density bonuses in preapplication conferences.

EL-LU-5.g

Develop objective design standards for residential and mixed-use development to provide a streamlined approval process; ensure compatibility with existing community character; and ensure appropriate open space, buffers, and pedestrian improvements are provided.

EL-LU-5.h

Develop and maintain an inventory of County-owned surplus lands with residential development potential and post the inventory on the County's website to inform developers of potential building opportunities.

Goal EL-LU-6

Commercial and mixed-use areas that serve community needs and support a robust local economy.

Policies**EL-LU-6.1**

Encourage centrally located businesses and services in places that conveniently serve residential areas and/or support revitalization or creation of town centers.

Economic development is addressed in more detail in the Economic Development Element.

EL-LU-6.2

Encourage mixed-use development with commercial and residential components in commercial areas when adequate infrastructure is available or can be made available.

EL-LU-6.3

Encourage conversion of existing commercial areas to a mix of uses to reduce travel distances and create neighborhoods that offer a variety of housing options.

EL-LU-6.4

Continue to allow parking reductions for residential uses in mixed-use areas where it can be demonstrated that shared residential and commercial parking will not result in an unacceptable impact on parking in the public right-of-way or off-street parking lots.

EL-LU-6.5

Encourage and support the establishment of more restaurants, retail, and other commercial services.

EL-LU-6.6

Encourage development of smaller neighborhood-supporting retail and services, including those offering local agricultural sales, to meet the everyday shopping and personal needs of the surrounding neighborhood.

EL-LU-6.7

~~Encourage development of larger regional commercial centers within Community Growth Boundaries that meet resident and visitor needs, such as supermarkets, restaurants, personal services, retail sales, professional offices, medical services, and other complementary uses, as well as venues for selling locally produced agricultural and related products.~~

EL-LU-6.8EL-LU-6.7

Discourage strip commercial development, allowing it only when alternative layouts are infeasible at the project site.

EL-LU-6.9EL-LU-6.8

Locate commercial service businesses, such as warehouses, repair services, and building materials sales, away from housing, schools, and other sensitive receptors. Use the following criteria for siting commercial service areas:

- Provide good access to highways or major collectors;
- Buffer existing or planned residential areas, schools, and other sensitive uses;
- Develop in depth rather than in a strip fashion along the access road to provide adequate room for parking, buffering, and other needs; and
- Encourage development as integrated planned areas in conjunction with community commercial areas or with common architectural and site development features.

EL-LU-6.10EL-LU-6.9

Encourage members of the private sector to establish new commercial recreation opportunities and to rehabilitate and restore older resorts, including destination resorts, lakefront resorts, dance halls, health and athletic clubs, equestrian facilities, and recreational camps.

EL-LU-6.11

~~Require proposals for budget retailers and discount chains to include a community meetingMAC early in the proposal process to gather input from community members and incorporate refinements to the proposal in response to community concerns. Also require such projects to prepare an analysis of economic and blight-inducement impacts of the proposed development on retail businesses in the market area, employment, County revenues and services, and other relevant economic considerations. The County may waive the requirement for this analysis if the new establishment is found to offer a public benefit and is in alignment with the community's vision for its future development.~~

EL-LU-6.12EL-LU-6.10

Encourage opportunities for indigenous-owned businesses within mixed-use and commercial areas.

Actions

EL-LU-6.a

Update regulations for cannabis retail uses to maintain setbacks from sensitive receptors, including schools, parks, and residential neighborhoods.

EL-LU-6.b

Investigate options to provide permit technical assistance for older resorts, retreat centers, and underused commercial properties.

~~EL-LU-6.b~~ EL-LU-6.c

Amend the Zoning Ordinance to diversify the types of resort uses allowed and conditionally permit resort/recreational uses in commercial land use districts.

EL-LU-6.c ~~EL-LU-6.d~~

Devise and offer incentives for reconstruction and/or redevelopment of local historical resorts.

Goal ~~EL-LU-7~~ —

Industrial areas that are compatible with their surrounding environment while supporting the economy.

Policies

EL-LU-7.1

Encourage a wide range of industrial development activities, prioritizing industries that create living-wage jobs that maximize the education, skills, and talents of county residents.

The Economic Development Element provides more comprehensive guidance for supporting the local economy.

EL-LU-7.2

Encourage development of visually attractive, carefully planned industrial parks and planned developments in areas with suitable topography and adequate infrastructure.

EL-LU-7.3

Require new industrial or heavy commercial uses to be sited appropriately, on land with average slopes not exceeding 20 percent and outside of a hazard zone, preferably served by public sewer and water facilities and within a Community Growth Boundary.

EL-LU-7.4

Encourage industrial uses that use geothermal energy generated on-site when compatible with surrounding land uses.

EL-LU-7.5

Require a planned development process for permit approvals of any industrial or heavy commercial uses of five acres or more in size.

EL-LU-7.6

Require new industrial and heavy commercial uses to be designed to avoid adverse impacts to adjacent uses, particularly residential neighborhoods, schools, and other sensitive uses, with respect to air quality, noise, odor, vibration, and other impacts.

EL-LU-7.7

Allow asphalt batch plants and similar processing facilities that are directly associated with the development of a resource to be located at the source of the resource under the following criteria:

- Require a use permit;
- Prohibit any commercial or industrial uses that are not related to the processing of the resource; and
- Require mitigation for impacts to surrounding land uses, the environment, and Lake County watersheds to protect air, surface water, and groundwater quality.

EL-LU-7.8

Consider including a requirement for consultation with Tribal governments to include Tribal Cultural Resource analysis within the Cultural Resource Reports for discretionary projects.

Circulation

An effective transportation network promotes health, safety, and well-being of all communities by supporting:

- **Public health** in terms of how well it supports walking, biking, equestrian travel, and access to food, healthcare, public services, tribal community centers, and traditional gathering areas.
- **Safety** in terms of how efficiently community members can evacuate and first responders are able to reach destinations during emergency events, as well as how safe the network is for all users.
- **Personal and community economic stability** in terms of how well it supports access to employment opportunities, shopping, and services; distribution of locally produced products; and the ability of tourists to access the region's attractions, including tribal cultural tourism.
- **Equity** in terms of how easily community members of all ages, abilities, and income levels can access their daily needs.
- **Environmental quality** in terms of the amount of greenhouse gas (GHG) and air pollutant-emitting vehicle miles that are driven by both personal automobiles and trucks, and the availability of electric vehicle charging infrastructure or other amenities to support the use of active transportation and high occupancy vehicles.
- **Financial soundness** such that the community can afford to operate and maintain the network in a state of good repair with existing revenue sources and that any expansion has complete funding for capital costs and its long-term operations and maintenance (O&M) costs.



The Circulation Element supports mobility options for those driving, walking, and biking.

This Circulation Element aims to provide a transportation network that moves people and goods in and around the East Lake Communities Planning Area conveniently and with minimal risk of injury. The East Lake Communities Planning Area's circulation network is highly dependent on highway transportation. There is limited pedestrian and bicycle infrastructure, and, as with many rural communities, the Planning Area does not have extensive transit services. However, this element serves to optimize mobility options for bicyclists, pedestrians, and transit-dependent populations, like older adults, to the extent possible. The element also aims to support safe routes to school and access to key destinations, including opportunities for recreation.

The following sections describe the existing and planned roadway network in the Planning Area, including goods

movement, active transportation and transit networks, and aviation travel options, followed by emerging trends in circulation. The element concludes with a comprehensive set of goals, policies, and actions to optimize these networks in the years to come.

Roadway Network

Existing Roadway Facilities

Highways, roads, and streets are classified based on their functionality (i.e., the role they play in the overall roadway network). The East Lake Communities circulation network features local roads and streets, collectors, and arterials depicted in **Figures EL-4** (Existing Countywide Roadway Functional Classifications) and **EL-5** (Existing East Lake Communities Area Roadway Functional Classifications). **Table EL-2** describes major roadways in the East Lake Communities Planning Area and the adjacent City of Clearlake by their functional classification and **Table EL-3** shows Lake County road standards by functional classification.

The principal arterials that underpin the East Lake Communities Planning Area roadway system ~~is are~~ SR 20 and SR 53. ~~SR 20 traverses east to west in the Planning Area and serves regional and local traffic through the communities along Clear Lake, including Kono Tayee, Paradise Valley, Glenhaven, Clearlake Oaks, Double Eagle Ranch and Spring Valley.~~ SR 53 traverses north to south through the Planning Area and serves as a connection between SR 20, through the city of Clearlake, and south through the Lower Lake and Middletown Planning Areas. ~~The City of Clearlake has several minor arterials, including Lakeshore Drive and Olympic Drive. SR 20, a major collector road, traverses east to west in the Planning Area and serves regional and local traffic through the communities along Clear Lake, including Kono Tayee, Paradise Valley, Glenhaven, Clearlake Oaks, Double Eagle Ranch and Spring Valley. Other~~ Major collector roads in the East Lake Communities Planning Area include New Long Valley Road, North Drive, Lakeshore Drive, and Sulphur Bank Drive. Other roadways in the Planning Area are minor collectors and local County-maintained roads, ~~such as High Valley Road.~~

Roadway Classifications

Local roads and streets primarily provide traffic movement within neighborhoods and are generally low-speed, low-volume roads.

Collectors (major or minor) provide access between low-speed/low-volume local streets and high-speed/high-volume arterials. As their name implies, minor collectors collect traffic from the local streets and distribute it to the arterial system or to the smaller urban areas that they directly serve. Major collectors provide circulation between neighborhoods.

Arterials (principal or minor) primarily link cities and towns within and outside the state. Arterial roadways typically have two or more lanes and allow the highest speeds of any roadway functional class.

Figure EL-4 Existing Countywide Roadway Functional Classifications

Commented [AG3]: This figure has been updated



Table EL-2 Functional Road Classifications for Major Roadways in the Planning Area

Function	Number	Name	Limits
Principal Minor Arterial	SR 20	SR 20	US Highway 101 (Calpella) to I-80 (Crystal Lake)
Principal Arterial	SR 53	SR 53	SR 20 to SR 29 (Lower Lake)
Major Collector	SR 221	New Long Valley Road	Spring Valley Road to SR 20
Major Collector	SR 216	Sulphur Bank Drive	SR 20 to Country Club Drive
Major Collector	SR 209	North Drive	From Crestview Drive to Sulphur Bank Drive
Major minor Collector	SR 216	Sulphur Bank Drive	North Drive to SR 20
Major Collector	SR 206	Crestview Drive	North Drive to Lakeshore Drive
Major Collector	N/A	Lakeshore Drive	Crestview Drive to San Joaquin Ext.

Note: Excludes roadways in the City of Clearlake.

CR = County Road; I = Interstate; SR = State Route; NA = Not Applicable

Table EL-3 Lake County Road Standards

Standards	Principal Arterial	Minor Arterial	Major Collector	Minor Collector	Local
Speed Limit	60 MPH	50 MPH	40 MPH	30 MPH	20 MPH
Number of Lanes	2-4	2-4	2	2	2
Lane Width	12 ft	12 ft	12 ft	11 ft	10 ft
Right-of-Way Width	60 ft	60 ft	50 ft (minimum)	50 ft	40 ft
Maximum Grade	12%	12%	12%	12%	16%

ft = feet; MPH = miles per hour

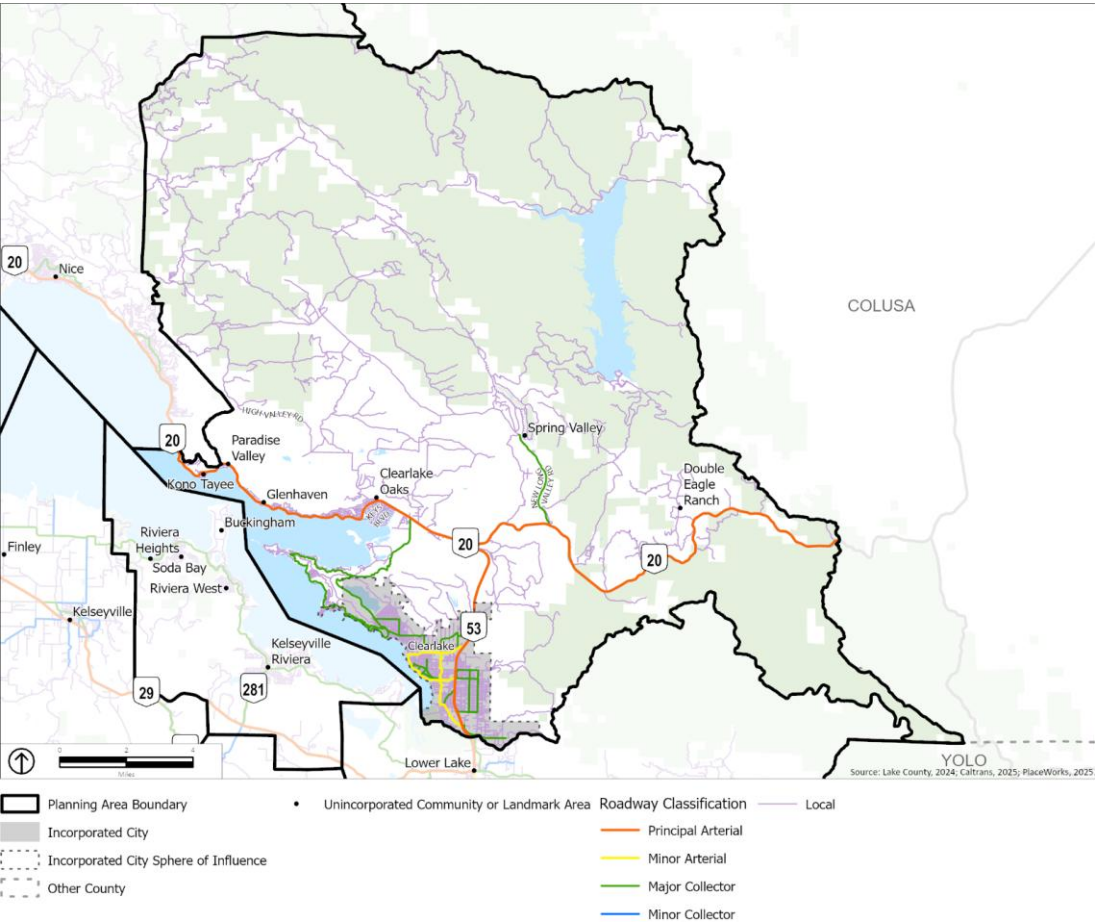
Level of Service

Traffic in Lake County fluctuates significantly by season due to the influx of tourists to the area in the summer months. While State facilities accommodate the highest volumes of traffic, several county roads that run through the City of Clearlake, including Lakeshore Drive, Olympic Drive, and Old Highway 53 also become busier during the summer.

Level of service (LOS) is used as a quantitative measure of roadway conditions based on the quality of their traffic operations from a driver's perspective. LOS considers conditions such as road configuration, traffic volume, speed, convenience, and capacity.

Figure EL-5 Existing East Lake Communities Area Roadway Functional Classifications

Commented [AG4]: This figure has been updated



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LOS is generally defined as follows under peak hour conditions (e.g., 7 to 8 a.m. or 5 to 6 p.m.):

- **LOS A** represents free-flow travel with an excellent level of comfort and convenience and the freedom to maneuver.
- **LOS B** has stable operating conditions, but the presence of other road users causes a noticeable, though slight, reduction in comfort, convenience, and maneuvering freedom.
- **LOS C** has stable operating conditions, but the operation of individual users is substantially affected by the interaction with others in the traffic stream.
- **LOS D** represents high-density, but stable flow. Users experience severely restricted speed and freedom to maneuver, with poor levels of comfort and convenience.
- **LOS E** represents operating conditions at or near capacity. Speeds are reduced to a low, but relatively uniform, value. Freedom to maneuver is difficult with users experiencing frustration and poor comfort and convenience. Unstable operation is frequent, and minor disturbances in traffic flow can cause breakdown conditions.
- **LOS F** represents congested conditions where traffic operates well below posted speeds, delays accumulate at intersections, and queuing forms at bottleneck locations. This condition exists wherever the volume of traffic exceeds the capacity of the roadway. Travel times become less reliable under LOS F conditions and drivers start adding buffer time to their trips to ensure being on time to appointments.

These definitions of LOS are contained in the Highway Capacity Manual (HCM). The HCM methodology is the prevailing measurement standard currently throughout the United States.

LOS expectations are used to determine specific roadway types and sizes (e.g., functional classification and number of lanes). Providing better LOS conditions comes with higher costs and other tradeoffs like higher energy consumption, emissions, and safety risks that must be balanced against the County's environmental goals. Stable LOS E conditions represent the most efficient use of the roadway space since the public roadway is operating near its capacity.

As part of the General Plan update process, the County conducted an analysis in 2025 of existing LOS and other circulation conditions for several roadways in the county. The nearby roadways studied, Olympic Drive, Lakeshore Drive, and Old Highway 53, operate at LOS D or better.

Vehicle Miles Traveled

If the number of residents, workers, and visitors increase in the county over time, more driving will occur. While some driving in and through the county is inevitable given the long distances between destinations in the county, the County and other agencies can take actions to concentrate growth within and near denser areas to help reduce the length of trips and to increase the viability of walking, bicycling, or taking transit. For example, the developed communities depicted in **Figure EL-1** are areas where land uses can be

located near enough to support walking or biking as a viable mode of transportation, thus reducing vehicle miles traveled (VMT). VMT is a metric that accounts for the number of vehicle trips generated and the length or distance of those trips. California law connects VMT reduction to the State's GHG reduction goals and other objectives, including the encouragement of public transit, infill development, and promotion of active transportation for public health. Though Lake County recognizes the importance of these objectives, the nature of the region's rural land uses with long distances between destinations limits the potential for VMT and GHG reduction through public transit or large mixed-use development projects.

VMT can be measured with a variety of methods and using multiple metrics. This General Plan relies on VMT estimates and forecasts from the Lake-Mendocino travel demand model to assess planning goals and environmental impacts.

Goods Movement

Many businesses and residents in the East Lake Communities Planning Area rely on goods movement for deliveries and importing or exporting products. Goods in the East Lake Communities Planning Area are primarily transported along SR 20 and SR 53, which are California Legal Advisory Routes. **Figure EL-6** shows the County's designated truck routes to accommodate large vehicles, including for goods movement.

Planned Roadway Network Improvements

The 2022 Lake County Regional Transportation Plan (RTP) is a State and federally mandated plan for transportation improvements in the county. The RTP is updated every four years by the Lake Area Planning Council for Lake County. The RTP is required to include a list of prioritized transportation improvement projects that are financially constrained or that have funding earmarked for the project as well as those that are financially unconstrained, meaning they do not have funding earmarked and do not have a specific timeline for implementation.

Truck Route Classifications

Truck route classifications are based on the allowable size and design specifications of truck tractors or semitrailers.

Terminal Access Routes allow STAA* trucks, which include semitrailers up to 53 feet long that meet several other conditions regarding distance between axles.

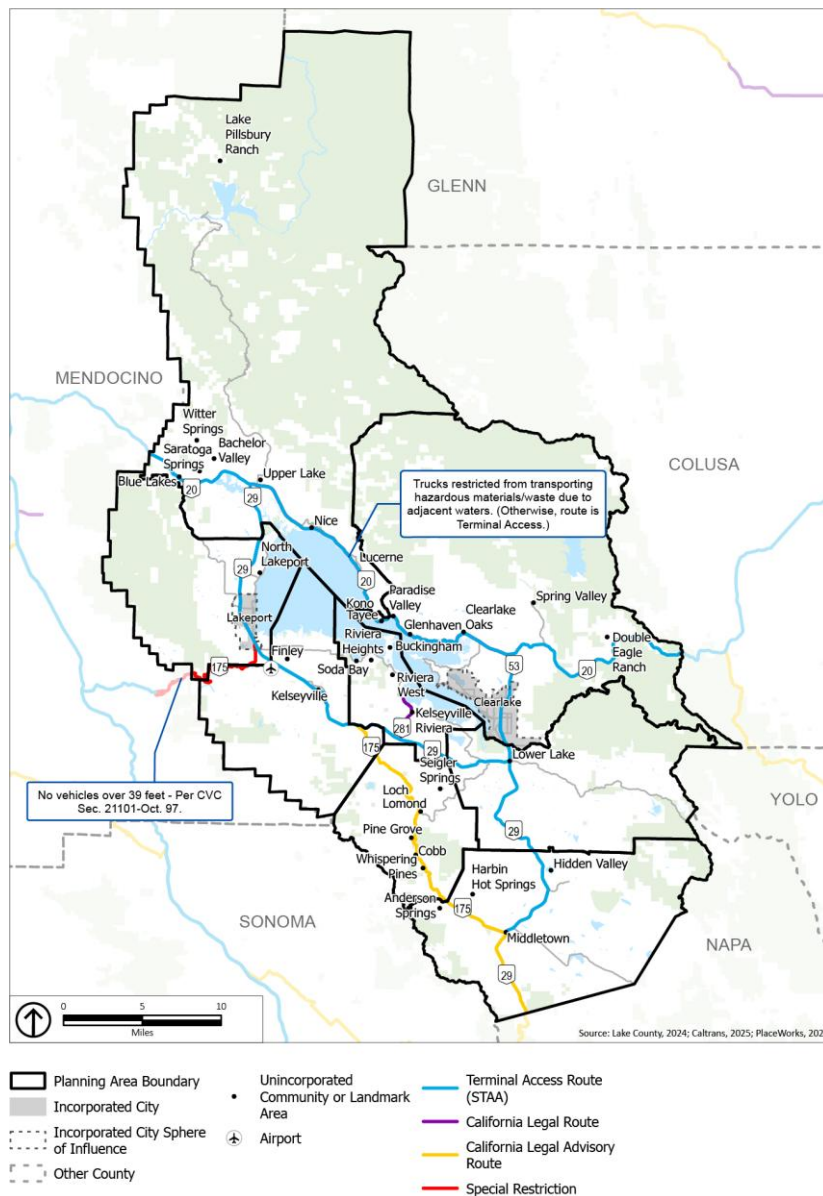
California Legal Routes allow California Legal trucks, which include truck tractors with total lengths of up to 65 feet (truck and semitrailer combined) or double trailers of up to 75 feet.

California Legal Advisory Routes also allow California Legal trucks but provide additional travel advisories for trucks that exceed posted kingpin-to-rear-axle lengths due to roadway turning radii.

Special Restriction Routes restrict certain trucks based on vehicle length or weight, cargo type, or number of axles.

*An STAA truck is one allowed to operate on the national network of highways due to the Surface Transportation Assistance Act (STAA) of 1982, which permits larger and heavier trucks to use certain federally aided highways, including the Interstate system and specific primary routes.

Figure EL-6 Countywide Designated Truck Routes



All planned roadway projects must undergo environmental review to identify potential project impacts to ecological, archeological, Tribal, and cultural resources, as well as VMT and other impacts, and establish measures to avoid or mitigate against impacts during the project implementation phase. New transportation projects must also undergo tribal consultation to identify and protect sacred sites, traditional use areas, burial sites, and archaeological resources. Mitigation measures, alternative alignments, and culturally sensitive design practices should be implemented where impacts cannot be avoided.

Figure EL-7 shows the County's planned roadway network improvements for both the East Lake Communities Planning Area and the adjacent City of Clearlake from the RTP, including the following projects:

- ~~Roadway widening on Lakeshore Drive between Olympic Drive and SR 53 in the City of Clearlake (financially constrained)~~
- ~~Roundabout at the intersection of Dam Road and Dam Road east of SR 53 in the City of Clearlake (financially constrained)~~
- ~~Roundabout at Lakeshore Drive and Olympic Drive in the City of Clearlake (financially unconstrained)~~
- Shoulder widening and safety improvements on SR 20 for a third of a mile between Morine Ranch Road and Whisner Lane (financially unconstrained)
- Intersection improvements at SR 20 and Whisner Lane south of Clearlake Oaks (financially unconstrained)

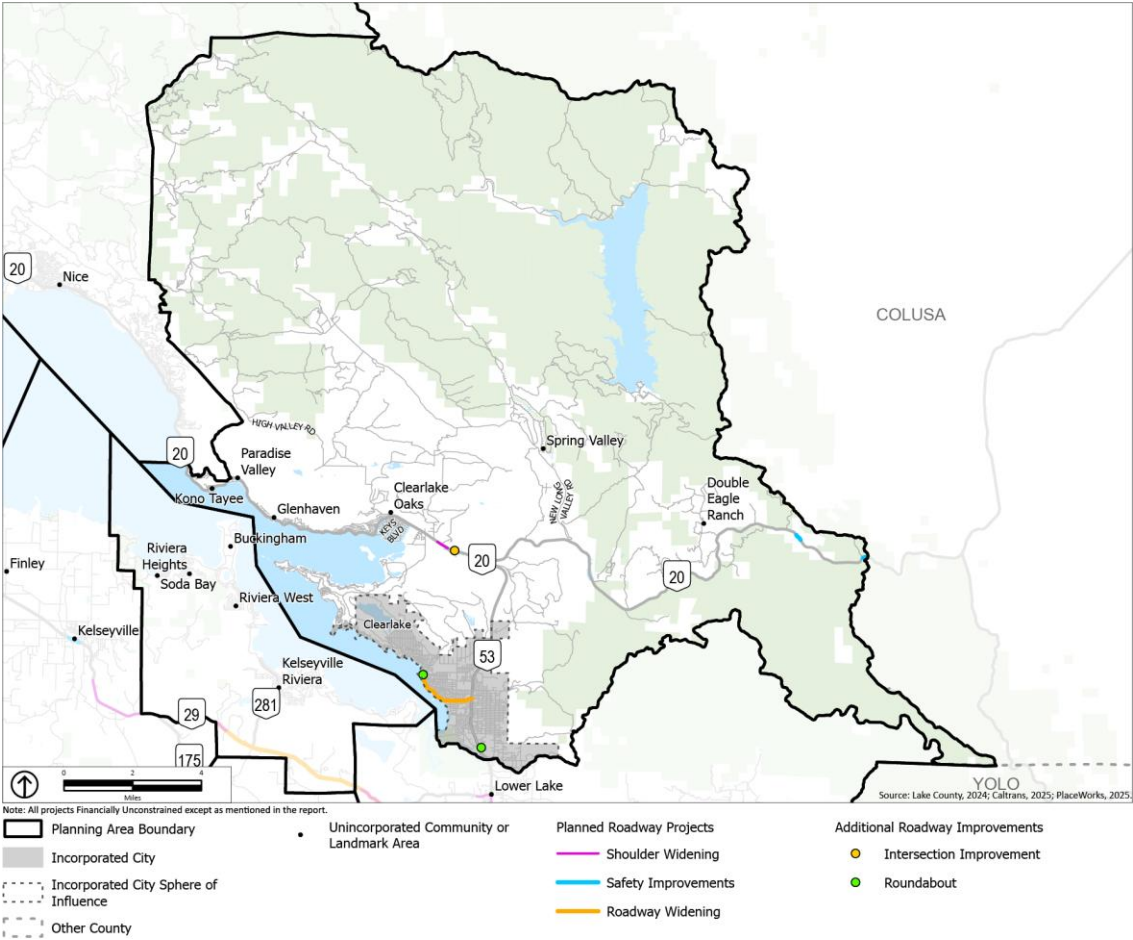
Active Transportation

Active transportation is defined as the transport of people or goods through non-motorized means, based around human physical activity. Nearly all the East Lake Communities Planning Area outside of the City of Clearlake lacks active transportation facilities, such as sidewalks, crosswalks, and bicycle lanes. The Open Space, Conservation, and Recreation Element includes information about recreational trails.

Existing Pedestrian Facilities

Most of the East Lake Communities Planning Area, such as Kono Tayee, Glenhaven, Spring Valley, and Double Eagle Ranch, consist of small residential areas with no sidewalks or marked crosswalks. The Clearlake Oaks community has several segments of sidewalk along SR 20, which serves as its main street, but similarly, residential areas are without sidewalks. ~~The City of Clearlake has a more robust walking network in key commercial areas of the city.~~

Figure EL-7 East Lake Communities Planning Area Planned Roadway Network



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Existing Bicycle Facilities

The California Streets and Highways Code Section 890.4 establishes three primary bikeway classifications, described in the text box. In addition to those listed, Class IV Separated Bikeways are classified in the California Vehicle Code, though none exist in the East Lake Communities Planning Area.

As shown in **Figure EL-8**, ~~Class II bike lanes in the City of Clearlake are present along Old Highway 53, Dam Road, Lakeshore Drive, and Olympic Drive.~~ The only bike facility in the unincorporated communities is a short segment of Class II bike lanes on SR 20 in the Clearlake Oaks community.

Planned Active Transportation Improvements

The Lake County Active Transportation Plan (ATP), integrated into the 2022 RTP, recommends additional class II and class III bike facilities in the East Lake Communities Planning Area. ~~and an enhanced network of walking and biking facilities in the adjacent City of Clearlake.~~ **Figure EL-8** shows the future bicycle and pedestrian facility improvements planned in the East Lake Communities Planning Area and the City of Clearlake from the County's 2022 RTP/ATP. All proposed sidewalks are in the City of Clearlake. **Table EL-4** lists the planned bicycle facilities in the unincorporated county. All planned projects shown are financially unconstrained (unfunded). While the County or other responsible agencies are actively seeking funding for several of these active transportation projects, none is identified, so the project may not be developed.

Bikeway Classifications

Shared-Use Paths (Class I), also called bike paths, these facilities provide a fully separated, paved facility for the exclusive use of bicycles and pedestrians. Shared-use paths have minimal cross flow with motorists and are typically along landscaped corridors.

Bike Lanes (Class II) are on-street facilities that use striping, stencils, and signage to denote preferential or exclusive use by bicyclists. Bike lanes are contiguous with motor vehicle travel lanes and should provide a minimum standard width of five feet for comfortable riding.

Bike Routes (Class III), a route for bicyclists designated by signs or optional pavement markings where bicyclists travel on the shoulder or share a lane with motorists. Bike routes are typically used on low-speed and low-volume streets to connect bike lanes or paths along corridors that do not have enough space for dedicated Class I or II facilities.

Table EL-4 Planned Bicycle Facilities

Class	Roadway	From	To	Length (Miles)
II	SR 20	Nice Lucerne Cutoff	Sulphur Bank Road	17.4
III	High Valley Road	Mendocino National Forest	Foothill Boulevard	4.2
III	Keys Boulevard	SR 20	End of Boulevard	1.1
III	Konocti View Drive	Lakeland Street	Keys Boulevard	0.1
III	Lakeland Street	Lake Street	Konocti View Drive	0.1
III	Sulphur Bank Drive	SR 20	City of Clearlake/ Country Club Drive	4.1

Note: Only showing facilities in the unincorporated county (excluding the City of Clearlake).

Source: County of Lake, 2022, *Lake County Regional Transportation Plan/Active Transportation Plan*.

Transit

Existing Transit Service

Lake Transit Authority (LTA) provides public transit services throughout Lake County and operates connecting routes to intercity and regional bus services in adjacent Napa and Mendocino Counties. As shown in **Figure EL-9**, the Planning Area is served by LTA Route 1. LTA's Routes 3, 4, 10, and 11 connect to Route 1 in the City of Clearlake and serve the Clearlake-Lower Lake area and other Planning Areas to the south and west.

LTA Route 1 provides 10 roundtrips on weekdays and 4 roundtrips on weekends along SR 20, linking communities along the north and east side of the lake, starting in north Lakeport, to Upper Lake, Nice, Lucerne, Glenhaven, Clearlake Oaks, and Clearlake. LTA Route 3 provides four roundtrips on weekdays and limited trips on weekends in Clearlake. It runs south along SR 29, circles around the Lower Lake community's downtown area, and continues south to Napa County through Calistoga, St. Helena, and Deer Park. LTA Route 4 provides seven roundtrips on weekdays and limited trips on weekends along SR 29, connecting Clearlake, Lower Lake, and Lakeport. LTA Routes 10 and 11 run exclusively in the City of Clearlake every hour from 6 a.m. to 7:30 p.m. on weekdays and 9 a.m. to 5 p.m. on weekends.

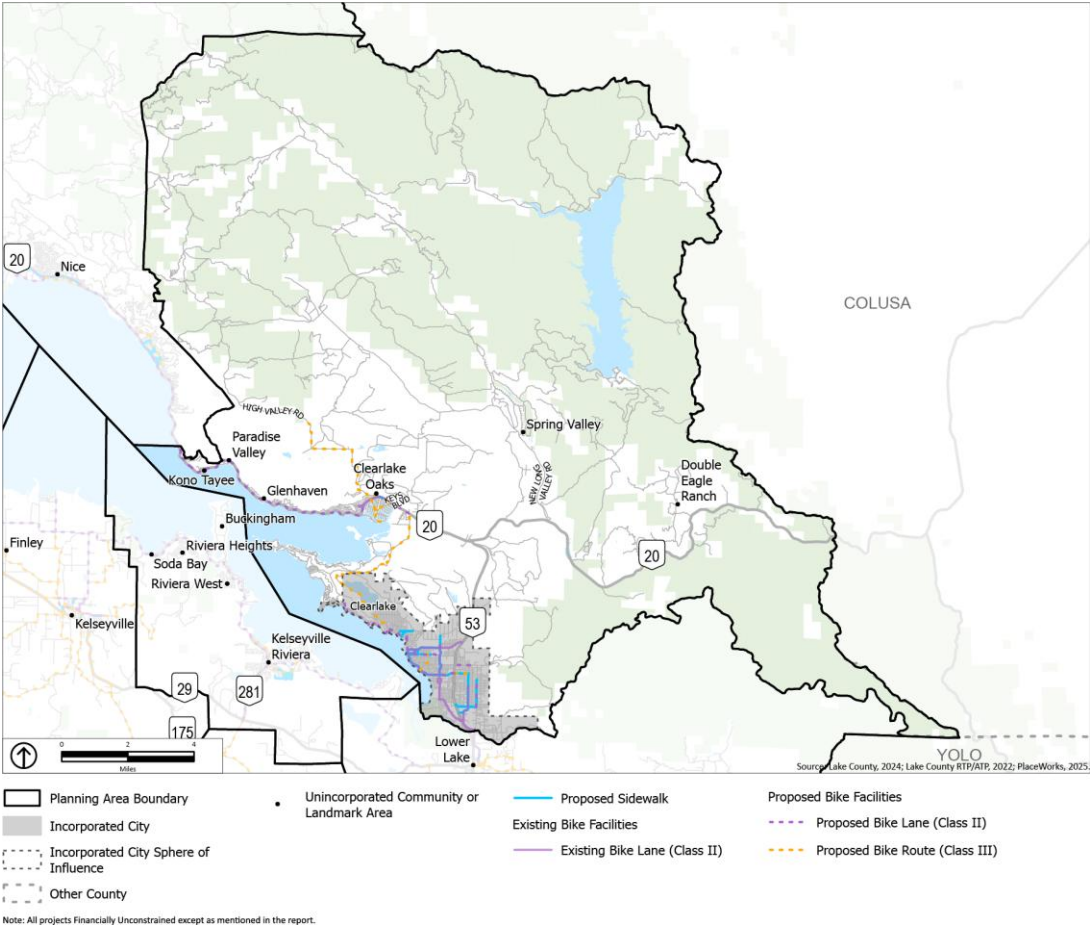
In addition to fixed routes, LTA offers Dial-a-Ride services near the City of Clearlake, as shown in **Figure EL-9**. In areas not served by Dial-a-Ride, LTA offers Flex Stop service where the bus will travel up to one mile beyond its regular route to provide more convenient service, though it must be reserved at least a day in advance.

LTA also offers Medi-Links, which provides nonemergency medical transportation throughout Lake County as well as out-of-county destinations such as Deer Park/St. Helena, Napa, Santa Rosa, Ukiah, Willits, and the Sacramento and San Francisco Bay areas. Medi-Links is a shared-ride service and must be arranged at least seven days prior to a medical appointment.



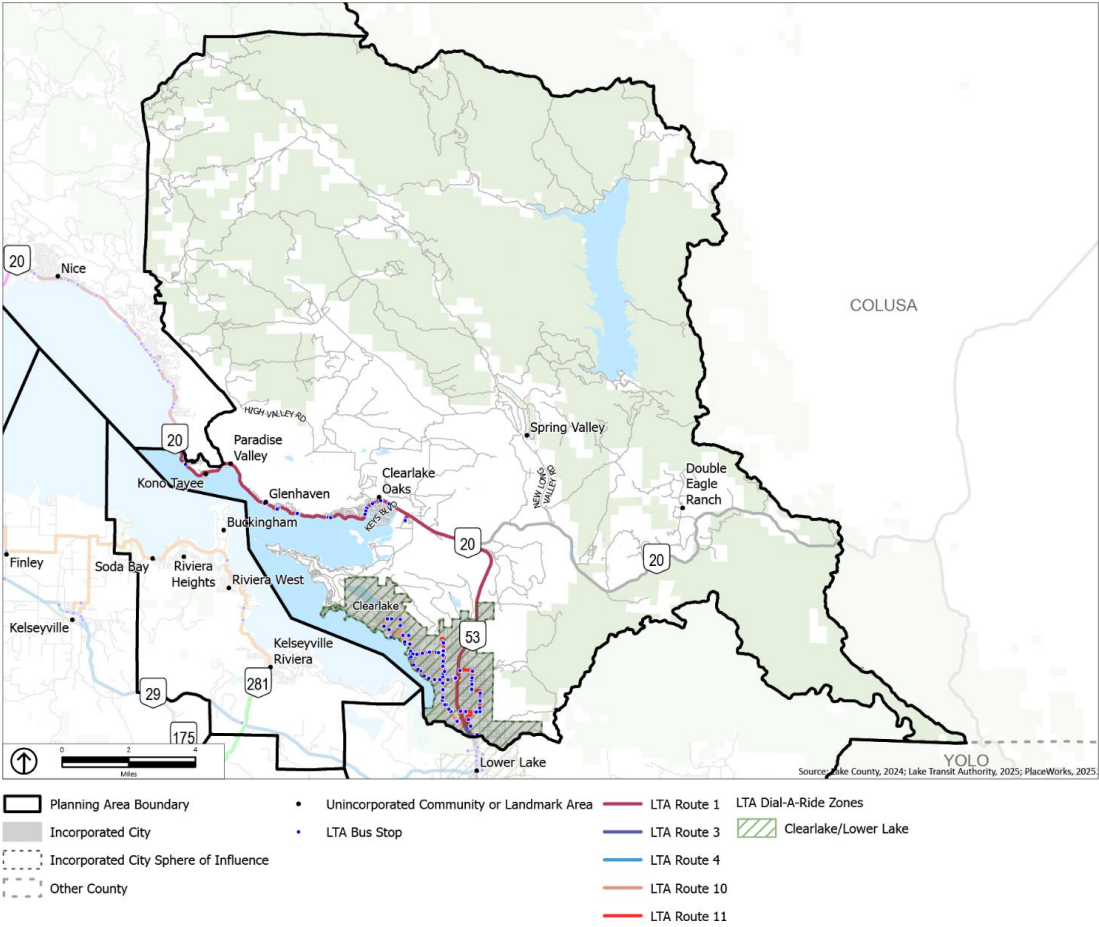
Transit service supports access to work, school, and essential services.

Figure EL-8 East Lake Communities Planning Area Existing and Planned Active Transportation Facilities



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Figure EL-9 East Lake Communities Area Transit Service and Facilities



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Planned Transit Improvements

The 2023 Lake County Transit Development Plan analyzes current public transportation operations and identifies potential changes that could improve transit to better serve Lake County communities. All changes recommended in the service plan, including modifications to the Clearlake Dial-a-Ride and the addition of bus service on Sundays, have already been implemented.

~~LTA successfully won nearly \$13 million in grant funding from the Transit and Intercity Rail Capital Program (TIRCP) grant program, which will fund the construction of the new transit center in Clearlake, the purchase of four hydrogen buses with the necessary fueling/maintenance infrastructure, and workforce development in coordination with local community colleges. The new transit center is at the intersection of South Center Drive and Dam Road Extension in Clearlake.~~ Other financially constrained transit projects identified in the RTP include both short- and long-term projects that would likely benefit residents in the Planning Area, such as improving bus stop amenities for existing and new routes, operations facility/fueling infrastructure expansion, and purchase of new transit vehicles. Note that RTPs often contain optimistic funding projections that may not be consistent with current financial realities.

Aviation

There are no public airports in the East Lake Communities Planning Area but there are several private airfields. The nearest public airport is Lampson Field in the Kelseyville Planning Area between the City of Lakeport and Kelseyville.

Emerging Trends

Technological advancements have contributed to new options for moving people and goods around Lake County, these include:

- Commercial ride-sharing services (e.g., Uber, Lyft)
- Food and grocery delivery services (e.g., Grubhub, Uber Eats, DoorDash, or independent restaurants and grocery stores offering their own delivery services)
- Electric vehicles (EV) (e.g., cars, bikes, and scooters)

Ride-sharing services offer residents, workers, visitors, and students the choice to travel by automobile to their desired destinations. Given the distances between origins and destinations in Lake County, this service offers one of the fastest travel time options in the county for those that do not have access to a private vehicle, cannot drive, or prefer not to drive. These services contribute to countywide VMT and emissions but also offer access to a wide variety of destinations that would otherwise be difficult to access.

Food and grocery delivery services similarly offer more options for community members to meet their needs, while being cost prohibitive to some. In some cases, coordinated online shopping and on-demand delivery can reduce VMT compared with in-store shopping, but the reduction depends on several other factors.

Electric vehicles help reduce GHG emissions, but they still contribute to vehicle congestion and delay. This Circulation Element includes policies that support installation of EV charging infrastructure as feasible.

Other transportation technologies may use the Lake County transportation network in the coming years, including Autonomous Vehicles (AVs), also known as self-driving cars and trucks. As of 2025, AVs are being tested but are not yet in widespread use, though many predict that they will be by 2050. AVs would lower the cost of vehicle travel, which could contribute to sprawl and greater levels of vehicle use. However, they could also lower the risk of collisions from human factors like driving under the influence, falling asleep, being distracted, and speeding and would allow more people that do not have access to private vehicles to travel more frequently at automobile speeds. In anticipation of AVs, this General Plan focuses on supporting growth suitable to the rural character of Lake County.

Costs and Revenues

Transportation costs and funding are important considerations for the General Plan. The Circulation Element should be financially constrained for capital, operations, and maintenance costs. As such, this element needs to include appropriate policies and actions that will align transportation network performance expectations with reasonable funding expectations. The County's ability to expand the transportation network to accommodate new growth will depend on its ability to fully pay for the ongoing operation and maintenance of the network.

There are 27 projects planned in the Lake Area Planning Council's RTP, outlining a variety of operation and maintenance projects aimed at improving the existing transportation infrastructure. Funding relies on multiple sources, such as State and federal grants and the Road Maintenance and Rehabilitation Account.

The County does not have sufficient funding to maintain the existing network in a state of good repair. A good example of the current challenge is the cost and funding of pavement. This is one of the larger maintenance costs for the County and does not include other transportation infrastructure or services such as transit services and facilities, sidewalks, bike paths, signals, markings, signs, or storm drains.

Goals, Policies, and Actions

Goal EL-C-1

Effective intergovernmental communication and cooperation on transportation issues.

Policies

EL-C-1.1

Coordinate with the incorporated Cities of Clearlake and Lakeport on regional transportation planning to support the goals of the Regional Transportation Plan.

EL-C-1.2

Maintain a cooperative relationship with the California Department of Transportation (Caltrans) to provide maintenance and safety improvements on State highways.

Actions

EL-C-1.a

Work with Caltrans and other transportation agencies to maintain the Regional Transportation Plan, identify funding priorities, and develop an expenditure plan for available transportation funds in accordance with federal, State, and regional transportation planning and programming procedures.

EL-C-1.b

Work with local agencies to identify road improvement opportunities.

EL-C-1.c

Maintain working relationships with transportation agencies in the region to ensure coordination of road improvements, data exchange on projected traffic impacts, and County participation in regional transportation planning activities.

EL-C-1.d

Work with Lake County Transit to expand Dial-a-Ride services, particularly for Equity Priority Populations.

EL-C-1.e

Support efforts to ensure transportation provider services are online-accessible to enhance utilization of current transit providers for out-of-county transportation needs.

Goal EL-C-2

A unified, coordinated, and cost-efficient road and highway system that is safe, uncongested, and efficient.

Policies

EL-C-2.1

Plan and construct a road network to accommodate projected growth in traffic volumes resulting from residential, commercial, industrial, and other development.

EL-C-2.2

Maintain, and improve where feasible, the road system to reduce traffic accidents, improve circulation, and maintain or improve its physical condition, in conformance with the priorities and recommendations established in the Regional Transportation Plan.

EL-C-2.3

When undertaking roadway improvements, prioritize maintenance and safety, considering both public reports and internal expertise and data.

EL-C-2.4

Prioritize roadway improvement or maintenance projects as follows:

- **Priority One: Urgent Projects.** Projects of an urgent nature that are clearly needed to protect the health and safety of the traveling public, such as imminent bridge or roadbed failure.
- **Priority Two: Safety Projects.** Projects that are intended to reduce the number and severity of collisions in a particular location.
- **Priority Three: Reconstruction/Maintenance Projects.** Projects that involve ongoing maintenance, rehabilitation, and reconstruction requirements to preserve the existing bridge and road network.
- **Priority Four: Capacity Improvements.** Projects that involve operational improvements to the existing road network that increase service efficiency and capacity.
- **Priority Five: New Construction Projects.** Projects that involve construction of a new bridge or road. New County construction projects will receive higher priority when they accommodate development in locations within reasonable proximity to employment and shopping centers and encourage energy conservation in the transportation sector.

EL-C-2.5

Work with Caltrans to support maintenance and improvements to Caltrans-managed roadways to support these roadways operating at LOS D or better. ~~However, Caltrans no longer receives funding to maintain operational levels of service and instead receives funding that support strategies to minimize vehicle miles traveled and maximize the safety of all road users.~~

EL-C-2.6

To maintain traffic flow and minimize potential collisions, only allow access from individual parcels to highways and arterials when access to lower road classifications is not feasible, as determined by the County. The County is best able to assess the feasibility of making roadway connections to local or lower order roads; however, Caltrans right of way designations and permit requirements will govern the ability of applicants to obtain Caltrans encroachment permits.

EL-C-2.7

Require roadway facilities constructed or used by new development to comply with the Lake County Road Standards (**Table EL-3**).

EL-C-2.8

Require new roads in residential areas to:

- Be designed to avoid cut-through traffic through the residential area.
- Be paved when serving more than four lots.
- Be improved with processed gravel at a minimum when serving four or fewer lots at densities less than one dwelling unit per five acres.
- Offer adequate right-of-way to contain road improvements for dedication.

EL-C-2.9

Work with California Highway Patrol and Lake County Sheriff to limit truck traffic to designated truck routes, as shown in **Figure EL-6**.

EL-C-2.10

Support the formation of maintenance districts to fund road maintenance and improvements in a manner consistent with County standards and when there is demonstrated community support.

Actions

EL-C-2.a

Implement and maintain the Capital Improvement Plan to ensure it continues to meet the needs of the East Lake Communities Planning Area as they evolve.

EL-C-2.b

Improve and construct roads to the Lake County Road Standards, as shown in **Table EL-3**. Use the American Association of State Highway and Transportation Officials (AASHTO) standards, Caltrans standards, and County expertise to inform any revisions to the Lake County Road Standards.

EL-C-2.c

Explore options to implement a traffic mitigation fee that would apply to new development and/or subdivisions for the maintenance of existing County roads.

EL-C-2.d

Implement vehicle weight limit restrictions on roadways near sensitive uses, like schools and residential neighborhoods, to discourage cut-through truck traffic.

EL-C-2.e

Pursue all available sources of funding and protect existing sources of funding for the development, improvement, and maintenance of the existing roadway system.

EL-C-2.f

Consult local Tribal governments when reviewing capital improvements for Tribal Cultural Resource impacts.

EL-C-2.g

Pursue opportunities for native plant restoration and habitat protection along rights-of-way and medians.

Goal EL-C-3

A transportation network that accommodates new development while reducing per-capita VMT and GHG emissions and promotes environmental protections and stewardship.

Policies

EL-C-3.1

Incorporate complete streets principles and practices into the planning, design, and construction of circulation network improvements to accommodate all network users, including people walking, biking, and riding transit.

“Complete Streets is an approach to planning, designing, building, operating, and maintaining streets that enables safe access for all people who need to use them, including pedestrians, bicyclists, motorists and transit riders of all ages and abilities.”

- Smart Growth America, 2025

EL-C-3.2

Lead and encourage activities to design and construct streets to support connectivity and promote pedestrian comfort and safety by including appropriate amenities, such as sidewalks on both sides of the street, shade trees, public benches, and pedestrian-scale lighting, when feasible, prioritizing core community areas.

EL-C-3.3

Encourage vehicle trip reduction through home occupations and telecommuting.

Air quality, a topic closely related to VMT and GHG emissions, is covered in the countywide Health and Safety Element.

EL-C-3.4

Encourage regional employers to provide employees with options and incentives to use alternative modes of transportation to work or to otherwise reduce VMT (e.g., transit subsidies, bicycle facilities, ridesharing, alternative work schedules, telecommuting, and preferential parking for carpools).

EL-C-3.5

Require new land use development projects to reduce VMT through feasible California Air Pollution Control Officers Association (CAPCOA) on-site VMT reduction strategies.

EL-C-3.6

Support transitioning on-road vehicles, including personal, business, government, and public transit fleets, to electric power from renewable sources or other zero-emission fuels.

EL-C-3.7

Support efforts to build EV charging stations at employment and community destinations, as allowed by market conditions.

EL-C-3.8

Encourage the use of road construction methods that reduce air, water, and noise pollution associated with road and highway development.

EL-C-3.9

Avoid locating roadways on naturally occurring asbestos whenever feasible alternative locations exist. If locating a roadway on asbestos is unavoidable, ensure adequate construction and surfacing with non-asbestos materials in compliance with local and State requirements.

EL-C-3.10

Evaluate impacts to tribal cultural sites and consult with local Tribal governments when planning street, transit, and roadway projects that could impact tribal cultural sites.

Actions**EL-C-3.a ^{*2}**

Adopt and maintain a VMT environmental threshold and development project screening process based on the Lake Area Planning Council Senate Bill 743 Vehicle Miles Traveled Regional Baseline Study.

EL-C-3.b ^{*}

Require new development with the potential to increase VMT to prepare a VMT analysis and to reduce VMT through feasible California Air Pollution Control Officers Association (CAPCOA) on-site VMT reduction strategies, as needed, to be consistent with the County's adopted VMT threshold.

² General Plan policies and actions marked with an asterisk (*) denote policies and actions that work to mitigate effects of cumulative development expected during the life of the General Plan, identified in the General Plan Environmental Impact Report (EIR).

EL-C-3.c

Develop a program to encourage trip reduction measures among County employees using various strategies, such as transit and/or carpool/vanpool subsidies, bicycle facilities, alternative work schedules, ridesharing programs, telecommuting options, employee education, and preferential parking for carpools/vanpools.

EL-C-3.d

Encourage carpooling by providing information to community members about park-and-ride locations and available programs.

EL-C-3.e

Amend Article 46 of the Zoning Ordinance to create electric vehicle charging and parking requirements for new development (e.g., one electric vehicle space per every 15 non-electric vehicle spaces and one charging station for large development based on square footage).

EL-C-3.f

Encourage and support annual maintenance to unpaved rural, ranch, and fire roads.

EL-C-3.g

Coordinate with local Tribal governments [and local community organizations](#) when siting public EV charging infrastructure, roads, or transit facilities.

Goal EL-C-4

A transportation system that protects human life.

Policies

EL-C-4.1

Implement a safe systems approach to designing the transportation network for all users; this approach acknowledges that people make mistakes and focuses on roadway design and operational changes to create safe streets.

EL-C-4.2

Strive to eliminate fatal and serious injuries for all road users by 2050 through a holistic view of the roadway system that includes provisions that account for human error, recognize vulnerable road users, such as pedestrians and bicyclists, and promote redundant and proactive safety measures.

EL-C-4.3

Require safety improvements, such as reflective signage and street lighting (where appropriate) to reduce the likelihood of collisions during any roadway infrastructure improvement process, [while avoiding offsite light impacts, including dark skies best practices](#).

EL-C-4.4

When modifying the transportation network, consider opportunities to expand emergency access so that all residential areas have at least two means of emergency access.

EL-C-4.5

Promote slow travel in neighborhoods through installation of infrastructure associated with reduction in crash risk, such as marked crosswalks, new lighting, and improved intersection controls.

EL-C-4.6

Pursue funding for a Safe Routes to School program and implement improvements, such as traffic-calming measures and high-visibility sidewalks and crosswalks.

EL-C-4.7

Engage local Tribal governments and local community organizations in safety planning and emergency access identification.

EL-C-4.8

Work with community groups to evaluate and address needs for traffic calming measures where appropriate. Prioritize traffic calming measures that do not cause additional noise impacts near residential areas.

Actions

EL-C-4.a

Identify safety corridors and areas that generate high concentrations of bicyclists and/or pedestrians. In these areas, use evidence-based strategies, including strategies identified in the United States Department of Transportation's Strategic Highway Safety Plan, to develop safety measures specific to those areas that are intended to eliminate traffic fatalities, with an emphasis on preventing fatalities of bicyclists, pedestrians, and users of other micromobility devices.

A **safety corridor** is a roadway segment within an overall roadway network where the highest number of serious injury and fatality crashes occur.

- *California Vehicle Code Section 22358.7(a)(1)*

EL-C-4.b

Regulate the location of private airfields and heliports to protect public safety and minimize impacts on nearby residents and other sensitive uses.

EL-C-4.c

Engage local Tribal governments when developing safety measures for high-risk corridors near tribal lands.

Goal EL-C-5

A safe and efficient public transportation system that offers a convenient alternative to the private automobile, reduces roadway congestion, and meets the community's transit needs.

Policies

EL-C-5.1

Support public transportation programs that promote access to shopping, employment, education, health care, and recreation.

EL-C-5.2

Support the continuation and coordination of transportation programs provided by social service agencies, particularly those serving older adults, individuals with disabilities, and other Equity Priority Populations.

EL-C-5.3

Support Lake Transit Authority on improvements to fixed-route bus and dial-a-ride services, including expanding the service area—prioritizing service in the community of Spring Valley, providing more frequent buses at key times of the day, and improving transit amenities, such as bus shelters, benches, and wayfinding signage at bus stops.

EL-C-5.4

Prioritize support for the following transit service efforts outlined in the Regional Transportation Plan and Lake County Transit Development Plan:

- Provide new and improve existing transportation services for older adults, individuals with access and functional needs, low-income individuals and/or other vulnerable populations;
- Provide intercommunity services between communities in the Planning Area and throughout the county; and
- Expand intercity transit carriers, particularly to Ukiah and the Bay Area.

EL-C-5.5

Encourage transit access and compatibility during the review and approval process for industrial, commercial, and residential development.

EL-C-5.6

Encourage bus stops at key destinations, population centers, and/or along arterials and collectors to facilitate public transit use.

EL-C-5.7

Engage local Tribal governments [and local community organizations](#) in planning routes and transit services, particularly for elders and community centers.

Actions

EL-C-5.a

Cooperate with the Lake Area Planning Council, Lake Transit Authority, and other related agencies in studying transit needs and developing an implementation program for public transportation services in the county.

EL-C-5.b

Explore opportunities to improve Lake Transit Authority services to Planning Area residents during updates to the Lake County Transit Development Plan, including regional connections to major destinations like the Sacramento International Airport.

EL-C-5.c

Update the Transit Development Plan with input from local Tribal governments to include tribal community needs.

Goal EL-C-6

A continuous and easily accessible active transportation system that facilitates safe, equitable, and connected walking, biking, and micromobility options.

Policies

EL-C-6.1

Construct new and maintain and improve existing pedestrian and bicycle facilities, consistent with the countywide Active Transportation Plan, as funding is available.

EL-C-6.2

Incorporate facilities for non-motorized users, such as sidewalks and bike routes, when constructing or improving roadway facilities and when reviewing new development proposals.

EL-C-6.3

Require bicycle lanes, multiuse paths, and sidewalks in conjunction with road improvement projects, as feasible. Develop, as funds become available, Class II bicycle lanes and multiuse paths on new collector and arterial streets.

EL-C-6.4

Require facilities for nonmotorized users, such as sidewalks and bike routes, for subdivisions ~~with a density of one or more dwelling units per acre~~ within Community Growth Boundaries. Required facilities should be commensurate with the impacts of the project, consistent with local and regional plans, and provide for infrastructure that integrates circulation and recreational use and aligns with surrounding development.

EL-C-6.5

Require new residential and commercial development to incorporate bike lanes and sidewalks, as appropriate.

EL-C-6.6

Require public and private development projects to include bicycle access and safe bicycle parking facilities at office buildings, schools, shopping centers, and parks, where feasible.

EL-C-6.7

Follow the adopted Caltrans design standards for the development, maintenance, and improvement of bicycle facilities.

EL-C-6.8

Construct and maintain bicycle routes and multiuse trails in a manner that minimizes conflicts between bicyclists, pedestrians, and equestrian users with automobiles and private property rights.

EL-C-6.9

Prioritize active transportation improvements benefitting tribal communities and Equity Priority Areas.

EL-C-6.10

Incorporate Tribal Ecological Knowledge in the design of active transportation improvements.

EL-C-6.11

Ensure new pedestrian/bike paths avoid or mitigate impacts to Tribal Cultural Resources, when feasible.

Actions**EL-C-6.a**

Engage in each update to the countywide Active Transportation Plan, prepared by the Lake Area Planning Council, to ensure the active transportation needs of the East Lake Communities Planning Area are met.

EL-C-6.b

Pursue funding to improve and maintain the existing bicycle system and to plan and construct new bicycle facilities that encourage commuting and recreation.

EL-C-6.c

Pursue funding to eliminate gaps in the existing sidewalk network and/or upgrade pedestrian facilities, with priority given to facilities near schools, connections between residential areas and commercial centers, and facilities in Equity Priority Areas.

Open Space, Conservation, and Recreation

The Open Space, Conservation, and Recreation Element addresses a wide range of natural and cultural resources as well as public parks and recreation facilities. The specific topics covered in this element include:

- Biological Resources
- Cultural Resources, including Archaeological, Historic, and Tribal Cultural Resources
- Energy Resources
- Mineral Resources
- Open Space and Recreation
- Scenic Resources

Use of natural resources must be balanced with responsible management and conservation efforts to ensure their ongoing availability, quality, and reliability. The resources addressed in this element are among the East Lake Communities Planning Area's most sensitive assets to be conserved and sustained for future generations, like publicly accessible natural areas, healthy ecosystems, and historic and indigenous culture.

Lake County is also committed to improving government to government relations with all Native Sovereign Nations in the county. As part of this commitment, at the time of this plan release, the County is drafting a Co-Management Agreement for the Clear Lake watershed and basin to enter into a Memorandum of Understanding. The County also has been working on a Heritage Resource Treatment Plan drafted by the Habematolel Pomo of Upper Lake and Koi Nation of Northern California and forwarded to the other five Tribal governments. As of the publication of this General Plan, all three are still in the draft stage.

This element inventories these natural and built resources in the East Lake Communities Planning Area and provides goals, policies, and actions to manage their use for all current and future residents.

Biological Resources

Natural Communities and Habitats

Natural vegetation communities in the East Lake Communities Planning Area are diverse and support a range of wildlife. Their components are impacted by factors like water availability, soil depth and chemistry, slope, and climate. At higher elevations, like Bartlett Mountain, primary vegetation communities include mixed conifer, deciduous forest, and montane hardwood. Conifer communities are defined by their evergreen tree varieties, like firs and pines. Deciduous forests host trees that lose their leaves in winter, such as oaks and buckeyes. Montane hardwood forests contain both conifer and



Community parks, like Hinman Park in Nice, are part of the area's network of park and open space areas.

deciduous trees. The Planning Area's eastern and southern portions primarily consist of brush and chaparral communities, which are characterized by low-growing, dense, woody shrubs and brush-[interspersed with oak woodlands](#). Grasslands are most prevalent at Rattlesnake Island, a roughly 50-acre island offshore from Clearlake Oaks.

The Open Space, Conservation, and Recreation Element recognizes the ecological and cultural importance of the East Lake Communities Planning Area's native habitats. Through partnership with Tribal governments, Lake County will protect biodiversity, restore natural systems, and sustain traditional ecological and cultural relationships that have cared for these lands since time immemorial.

Wetlands and Riparian Corridors

Wetlands and riparian corridors (i.e., land areas bordering rivers, streams, and other water bodies) support the widest array and highest number of mammals, reptiles, birds, and amphibians of the vegetation communities in the Planning Area. Riparian areas also create a buffer between land and water, filtering out sediment, debris, nitrogen, pesticides, and other pollutants before they reach the water body, while also protecting against erosion and slowing floodwaters. In the East Lake Communities Planning Area, major riparian zones are primarily along North Fork Cache Creek, which runs all the way from the northern Planning Area boundary with North Lake Communities to the southern Planning Area boundary with Lower Lake, where it joins with Cache Creek. Other major riparian corridors in the Planning Area include Bartlett Creek, Long Valley Creek, Schindler Creek, and Wolf Creek.

Fresh emergent wetlands are common along the Planning Area's Clear Lake shoreline. Wetland vegetation includes soft, non-woody, and water-dependent plants, such as sedge, common cattail, and tule.

Ecologically Sensitive Resource Areas

There are a variety of ecologically sensitive resource areas in the county. These can include mountain regions, valleys, forest lands, riparian corridors, and

Ecologically sensitive resource areas are areas containing unique, representative, and/or sensitive habitats, communities, or ecological processes.

lakes, as well as habitats that support State or federally listed threatened or endangered species, or species that are under consideration for listing, including wetland, riparian, vernal pool, and oak woodland habitats.

Clear Lake and its tributaries throughout the county are an ecologically sensitive resource area as habitat for the Clear Lake hitch (*Lavinia exilicauda chi*). The Clear Lake hitch is only found in the Clear Lake watershed, including Clear Lake and its tributaries, such as Blue Lakes and Thurston Lake, due to geologic processes that have isolated the Clear Lake hitch ecosystem. Adult hitch migrate upstream in tributaries of Clear Lake to spawn each spring and then return to the lake. Though there were once an abundance of hitch, their population has significantly declined as the Clear Lake ecology has been altered and degraded. Historically, the hitch were a vital part of the lake's ecosystem; an important food source for numerous birds, other fish, and wildlife; and a staple food for the Pomo tribes of the Clear Lake region. This General

Plan establishes a 20-foot Resource Conservation buffer around Clear Lake and its tributaries, as shown on **Figures EL-LU-2** and **EL-LU-3**, to protect the area from development that might further impact the hitch | population.

Based on data from the U.S. Fish and Wildlife Service, California Department of Fish and Wildlife, and California Native Plant Society, the East Lake Communities Planning Area hosts habitats used by both plant and wildlife species that are considered rare, threatened, or endangered. Some species include the bent-flowered fiddleneck (*Amsinckia lunaris*), bottlebrush sedge (*Carex hystericina*), California satintail (*Imperata brevifolia*), and eel-grass pondweed (*Potamogeton compressus*). The California Natural Diversity Database has also mapped areas containing habitat for protected animal species, such as the northern spotted owl (*Strix occidentalis caurina*) and Townsend's big-eared bat (*Corynorhinus townsendii*). However, few individual animals have been located. Another protected species, the foothill yellow-legged frog (*Rana boylei*), has been documented in local creek drainages in the Planning Area.

Cultural Resources

The Clear Lake Basin, which encompasses the East Lake Communities Planning Area, is well-studied and considered one of the state's most important regions for cultural resources. The region contains numerous ancestral village sites, ceremonial areas, petroglyphs, trails, and historic structures, which remain vital to both the tribal and broader community's identity. For tribal communities, these cultural resources are not relics of the past but living connections to language, tradition, and spiritual practice. Many cultural sites are intertwined with natural features, such as lakes and creeks that hold ecological and ceremonial significance. The sites, structures, and artifacts found in this area contribute significantly to local character and culture and enable the present-day community to connect to the Planning Area's long history.

The protection, study, and enhancement of cultural resources allows for important research and education. State legislation and programs, including the California Historical Building Code and the 1976 Mills Act, aid in the protection of these significant resources. The California Historical Building Code ensures historic buildings are maintained and rehabilitated using historically sensitive construction techniques. The Mills Act provides property tax incentives to owners of qualified, owner-occupied, historical properties to maintain and preserve them in accordance with the Secretary of the Interior's Standards for the Treatment of Historic Properties. Direct coordination with Tribal governments and collaborative management approaches ensure that Tribal cultural landscapes are identified, protected, and interpreted in accordance with Indigenous values and laws. By studying and protecting the Planning Area's cultural resources, past cultural patterns and processes will continue to provide a better understanding of the area's history and enrich its present.

Archaeological and Historic Resources

The East Lake Communities Planning Area is an archaeologically rich and culturally sensitive region, containing a high density of prehistoric sites. Significant sites of archaeological and historic resources in the Planning Area include Rattlesnake Island, an archaeological site at Borax Lake, and the site of a former

Patwin village. An inactive historic mine, the Sulphur Bank Mine, is also listed as a California Historic Landmark, and discussed further in the Mineral Resources section.

Listed in the California Register of Historical Resources and eligible for the National Register of Historical Places, Rattlesnake Island is the largest island on Clear Lake. It is just offshore of Clearlake Oaks and has six recorded archaeological sites. The largest site represents the ethno-historic nineteenth-century village of Elem, which is considered by some members of today's Elem community to be sacred land and their ancestral place of origin.

Listed on the National Register of Historic Places since 1991, the Hodges Archaeological Site at Borax Lake is between Clearlake and Clearlake Oaks. Archaeologists discovered many fluted projectile points (i.e., spear and arrow tips typical of the Paleoindian Period) at the site in 1948, indicating the presence of important resources in the area, possibly linked with the unique mineral and ecological composition of Borax Lake. Archaeologists estimate that human activities at Borax Lake date back to between 11,000 and 13,000 B.C.

Kuy-kuy, a 200- to 300-year-old village, is one of several important archaeological sites of the Hill Patwin people that ancestrally inhabited the upper watershed of Cache Creek. The site is on a bluff overlooking Cache Creek and contains the remnants of a ceremonial house and several small dwelling structures. It has been on the National Register of Historic Places since 1972.

Tribes and Tribal Cultural Resources

The original stewards and historic inhabitants of the Planning Area are the Southeastern Pomo, Eastern Pomo, and Patwin Wintun. Prior to European contact, the Southeastern Pomo and Eastern Pomo inhabited the shores of Clear Lake. The area was densely populated as Clear Lake served as a consistent source of food. Other tribal groups in the Planning Area lived in smaller distinct territories. The Hill Patwin Wintun tribelets bordered the Pomo communities but lived in the more mountainous regions to the east of Clear Lake, generally near creeks and other watercourses. Descendants of these tribes maintain a living presence in the area, continuing traditional practices and stewardship of ancestral lands.

Today, the Elem Indian Colony of Pomo Indians of the Sulphur Bank Rancheria, which is south of the community of Clearlake Oaks, offers a range of assistance programs to tribal members, including higher education assistance, job training, and rental assistance. Elem Indian Colony is a federally recognized Native American Tribe. Two Lake County Tribal Health Consortium (LCTHC) clinics are in the Planning Area. The mission of LCTHC is "To promote the positive change in the physical, spiritual, emotional, and social health status of the American Indians/Alaska Natives and communities we serve through culturally sensitive health care services." LCTHC offers a range of care services, including medical, dental, pediatrics, obstetrics, and behavioral health. The consortium operates pharmacies and a Diabetes Specialty Care Clinic.

This rich tribal history and living culture are reflected in a range of historic and sacred sites and resources throughout the Planning Area. Tribal cultural resources often are less tangible than an object or a site itself. For example, sometimes the importance is tied to views of or access to a sacred site. Therefore,

consultation with culturally affiliated Indigenous Tribes is key to identifying Tribal Cultural Resources, as required by Assembly Bill (AB) 52. In addition, AB 52 also requires that local agencies evaluate and mitigate to the extent feasible a project's potential impacts to Tribal Cultural Resources.

Pursuant to Senate Bill (SB) 18, the County notified and consulted with local Tribal governments throughout the General Plan update process. The policies and actions in this element reflect that tribal consultation and commit the County to sustaining a collaborative relationship through implementation of the General Plan.

Energy Resources

The East Lake Communities Planning Area, which includes the communities of Kono Tayee, Paradise Valley, Clearlake Oaks, Spring Valley, Glenhaven, and Double Eagle Ranch, have a total population of approximately 4,490 people. The highest energy demand is from residential, hotel and resort, commercial, and recreational uses. Affordable and reliable energy is an ongoing priority issue for Planning Area residents.

Most of the Planning Area's electricity, like the county's, is supplied from the statewide electrical grid operated by Pacific Gas and Electric Company (PG&E). Recent data shows that PG&E's electric power mix is largely GHG-free and/or from renewable sources. In 2022, 49 percent was from non-emitting nuclear generation, 38 percent from eligible renewable resources (wind, geothermal, biomass, solar, small hydro), 8 percent from hydroelectric facilities, and 5 percent from natural gas. Throughout the county, most homes and businesses use on-site propane storage tanks as an additional fuel source, as PG&E does not supply natural gas in Lake County. The use of private solar and wind turbines is not widespread but is growing.

Electricity utilities throughout California, including PG&E, have begun to occasionally "de-energize," or turn off the electricity for power lines that run through areas where there is an elevated fire risk. This is intended to reduce the risk of damage to power lines and igniting a wildfire. These events, called public safety power shutoff (PSPS) events, result in a loss of power for customers served by the affected power lines. A PSPS event may occur at any time of the year, but they usually occur during high wind events and dry conditions. PSPS events can also create vulnerabilities for community members that lack backup power supplies and depend on electricity for heating or cooling homes and buildings, lighting, and internet, as well as people who depend on electrically powered medical devices.

Mineral Resources

The California Department of Conservation, Geological Survey (California Geological Survey) classifies lands into Aggregate and Mineral Resource Zones (MRZs) based on guidelines adopted by the California State Mining and Geology Board, as mandated by the Surface Mining and Reclamation Act of 1975 (SMARA; Public Resources Code Sections 2710 et seq.). Cities and counties are required to incorporate MRZs delineated by the State into their General Plans. According to the California Geological Survey, there are no MRZs delineated by the State in the Planning Area or Lake County as a whole.

Gravel mining operations in the Planning Area, active and inactive, are generally along the North Fork of Cache Creek near SR 20. Mine operators have historically extracted sand and gravel directly from creek channels and floodplains, called “in-channel mining.” Excessive in-channel mining over decades has caused severe creek degradation in the North Fork of Cache Creek, and this practice is no longer permitted.

Other mining activities in the East Lake Communities Planning Area include extraction of red rock (volcanic cinder deposits), which is used to manufacture concrete blocks, road surfacing material, and decorative rock. Two quarries operate just east of the community of Clearlake Oaks. The Lake County Aggregate Resource Management Plan identifies two potential quarry sites in the Planning Area: Round Mountain and Quackenbush Mountain. The Round Mountain site is made up of two adjacent sites just over a mile northeast of Clearlake Oaks at the east end of High Valley. One site contains the same red lava rock deposits currently mined in quarries east of Clearlake Oaks, and the other includes basalt deposits. The Quackenbush Mountain site is at the top of Quackenbush Mountain, immediately east of the City of Clearlake, and contains significant deposits of the volcanic rock basalt.

Conflicts between mining and urban uses throughout California led to the passage of the Surface Mining and Reclamation Act (SMARA), which established policies for conservation and development of mineral lands. It contains specific provisions for the classification of mineral lands by the State Geologist.

The 1992 Lake County Aggregate Resources Management Plan, provided as Appendix A to this General Plan, provides more detail on the mineral resources and mining operations in Lake County.

Sulphur Bank Mine

The inactive Sulphur Bank Mine is a 220-acre property on the shore of Clear Lake adjacent to the Elem Indian Colony of Pomo Indians of the Sulphur Bank Rancheria. As noted in the introduction to this chapter, from 1865 to 1871, the California Borax Company extracted sulfur for gunpowder production. Once the mine pivoted to mercury production in 1873, it was one of the largest producers of mercury ore in California until its closure in 1957. Today, the Sulphur Bank Mine is listed as a California Historic Landmark. Mine tailings, waste rock, and a 23-acre flooded open pit remain on the site today.

The United States Environmental Protection Agency (EPA) declared Sulphur Bank Mine a Superfund site in 1991. Studies identified contaminants, including mercury, arsenic, and antimony, that posed a variety of threats to human health and the environment. In the 1970s, the United States Bureau of Indian Affairs used contaminated mine wastes as residential and infrastructural construction material at the adjacent Sulphur Bank Rancheria, further compounding health inequities faced by the Elem Pomo as a result of colonization. Surface water and groundwater discharge from the mine property have also contaminated wetlands north of the mine property as well as Clear Lake itself in the past. The EPA has initiated various “Response Actions” since the mine’s declaration as a Superfund site in 1991. In fall 2023, the EPA issued its final plan to clean up Operable Unit 1, which includes the mine site area, residential soils north of the mine, and residential soils south of the mine. The Environmental Justice Element discusses documented impacts of the Sulphur Bank Mine to the surrounding community.

Open Space and Recreation

Open space and recreation areas are depicted in **Figures EL-10** (countywide) and **EL-11** (East Lake Communities Planning Area).

Aside from the vast expanses of State and federal public lands, the East Lake Communities Planning Area has five recreation areas totaling approximately 120 acres. Most parks in the Planning Area are in the community of Clearlake Oaks and are classified as local parks, meaning they are smaller and primarily serve the immediate community. Clark's Island is a 1.5-acre island in the northwestern part of the Clear Lake Keys accessible by footbridge from SR 20. The island features a trail and picnic benches and is popular for dog-walking and bird watching. Clearlake Oaks Beach is a lake-oriented park immediately south of Clark's Island on the shore of Clear Lake. It is just over one acre in size and is popular for fishing, paddle boating, and picnicking. Amenities at Clearlake Oaks Beach include a boat launch, picnic areas, bathrooms, and a parking lot. Clearlake Oaks Plaza and the adjacent Nylander Park are less than a quarter mile north of Clark's Island on the west side of SR 20, adjacent to the Live Oak Senior Center. Clearlake Oaks Plaza includes a gazebo, labyrinth, and bench seating. Nylander Park features a playground and picnic tables and is notable for its extensive tree canopy.



Alpine Park in Lucerne features a playground, beach access, and a breathtaking view of Clear Lake.

Helen Mitchum Park is in the Spring Valley community. The 117-acre linear park runs along a creek corridor and features a campground with bathrooms, showers, a frisbee golf course, and creek access. Access is limited and managed by the neighboring homeowner's association.

In addition to these existing parks, the County is in the process of planning for the development of the approximately 580-acre John T. Klaus Park in Clearlake Oaks at the intersection of SR 20 and SR 53. The Lake County Board of Supervisors approved the donation of the property in 2022. Per community input, the County has considered a range of program elements for the site, and the final concept design as of 2025 includes hiking trails, non-motorized biking trails, dog parks, horseback trails, walk-in camping, nature-themed playground, frisbee golf, environmental visitor center, caretaker home, and native demonstration garden. The target completion date for the park is 2027.

State and federal lands in and around the East Lake Communities Planning Area include Mendocino National Forest, [Berryessa Snow Mountain National Monument](#), and lands owned by the Bureau of Land Management (BLM). These areas incorporate a large portion of the northern and eastern Planning Area and provide many recreational opportunities for residents and visitors, including hiking, backpacking, camping, hunting, fishing, boating, off-road vehicle use, and horseback riding.

Figure EL-10 Countywide Open Space, Conservation, and Recreation Areas

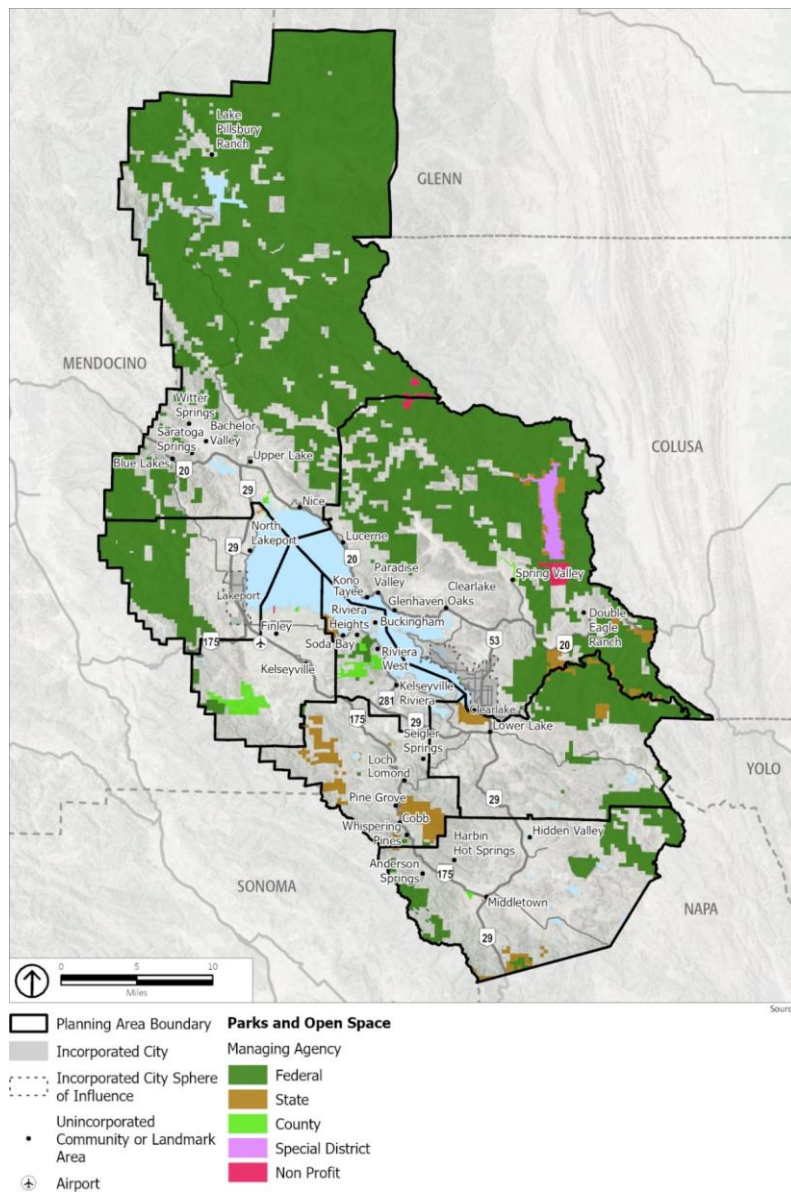
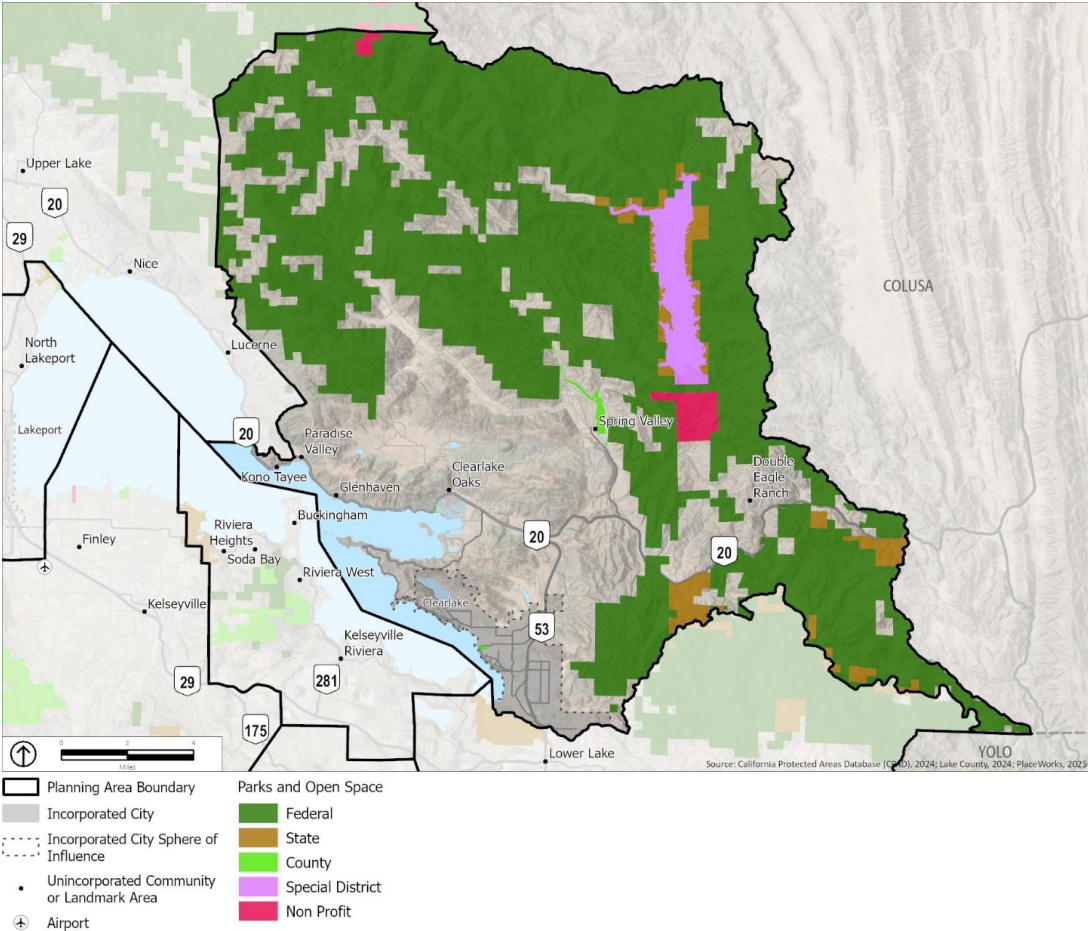


Figure EL-11 East Lake Communities Planning Area Open Space, Conservation, and Recreation Areas



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In 2005, the State of California designated 31 miles of Cache Creek, which spans both Lake and Yolo Counties, as a Wild and Scenic River System. This includes large portions of the North Fork Cache Creek in the Planning Area. Some segments are also designated as recreational areas. This classification provides access to special grants for plant and wildlife preservation and conservation activities.

The Lake County Parks, Recreation, and Trails Master Plan provides a roadmap for park, recreation, and trail improvements, addresses maintenance and operations of the system, and guides ongoing improvements and decisions, all informed by input from local residents, agencies, and environmental organizations. While the East Lake Communities Planning Area is well-served in terms of close-to-home recreation offerings, the plan outlines site specific improvements such as providing ADA paths at Clark's Island, investing in the pier and launch at Clearlake Oaks Beach, and increasing shade at Clearlake Oaks Plaza.

The Konocti Regional Trails Plan, adopted by the Lake County Board of Supervisors in January 2011, is a framework for developing a countywide network of non-motorized trails for activities like hiking, biking, and horseback riding while preserving natural and cultural resources. The Plan includes both water- and land-based trails along the SR 20 corridor in the East Lake Communities Planning Area. Overall, the purpose of the system is to highlight the region's beauty, wildlife, natural landscapes, and cultural history on trails along the ridgelines surrounding Clear Lake.

Scenic Resources

Scenic resources in the East Lake Communities Planning Area contribute substantially to residents' quality of life. They serve as a source of community pride, enhance property values, and offer beautiful and interesting views throughout the region. The Planning Area's historic character, forested ridges, grasslands and hills, and lake and mountain viewsheds are all valued scenic resources. The region's scenic landscapes and viewsheds also hold deep ancestral and spiritual significance to tribal communities. Protection of Tribal Cultural Resources within these visual corridors is essential to maintaining both the physical beauty and the Planning Area's cultural heritage.



Views of Clear Lake are an important scenic resource.

Views can be impacted by anything from billboards and utility lines to unmaintained structures and unscreened ~~industrial or~~ construction operations. Development on ridgelines and steep slopes can negatively impact scenic resources because development is often visible from a distance. Thoughtful policy, land use designations, and guidelines can preserve and improve a community's identity and maintain the quality of viewsheds.

Well-maintained scenic resources, especially scenic routes and highways, can bolster the Planning Area’s recreation-based economy, providing value for residents and visitors. Scenic routes are publicly accessible roadways that traverse picturesque landscapes. They offer extensive views of scenic areas or prominent features. The two primary highways in the East Lake Communities Planning Area, SR 20 and SR 53, are both eligible to be designated as State Scenic Highways. This is true for the entire lengths of both highways through Lake County. They offer a variety of vistas, from rolling hills and small valleys to the Clear Lake shoreline and Mount Konocti.

In addition to the routes in the Planning Area that are eligible as State Scenic Highways, several county roads have scenic and aesthetic value and have been identified as open space corridors of significance. These include:

- High Valley Road between Bartlett Springs Road to Clearlake Oaks, including the High Valley Road extension to SR 20
- Bartlett Springs Road from SR 20 to the Colusa County line
- Walker Ridge Road from Bartlett Springs Road to SR 20
- Sulphur Bank Drive from SR 20 to the Clearlake city limit
- New Long Valley Road from SR 20 to its terminus

To maintain the quality of life and the rural character of the East Lake Communities Planning Area, policy guidance in this section works to preserve and enhance scenic resources.

Goals, Policies, and Actions

Goal EL-OS-1

Protection of the Planning Area’s diverse biological resources, sensitive significant habitats, and ecosystem health.

Policies

EL-OS-1.1

Coordinate with applicable federal, Tribal, State, regional, and local land management agencies on natural resources and habitat planning, preservation, and mitigation measure development.

EL-OS-1.2

Support protection and restoration of wetland, riparian, and other significant natural areas in the East Lake Communities Planning Area for wildlife habitats, groundwater recharge, and passive recreation.

EL-OS-1.3

Protect natural lands, including natural resource areas, wildlife habitat, and open space, from encroachment or destruction by incompatible development and invasive species.

EL-OS-1.4

Require development patterns and practices that conserve habitat for protected species and sensitive biological resources, limiting encroachment into areas that support sensitive habitats and directing development into less-sensitive areas.

EL-OS-1.5

Maintain ecologically sensitive resource areas in their natural state to the greatest extent possible. Limit development in and near these areas to compatible, low-intensity uses with adequate provisions to protect sensitive resources, including setbacks around resource areas. Prohibit projects that would lead to fragmentation of ecologically sensitive resource areas.

EL-OS-1.6

Require the use of conservation easements, transfer of development rights, mitigation banking, or similar equity transfer documents to protect unique vegetation, habitat areas, habitat corridors, and sensitive biological resources in new projects.

EL-OS-1.7 *

Require a qualified biologist to prepare a biological resource assessment as part of project approval for proposed development on sites that may support special-status species, sensitive natural communities, important wildlife corridors, or regulated wetlands and waters to identify potential impacts and measures for protecting the resource and surrounding habitat. Require a qualified biologist conduct focused surveys, as identified and recommended in the biological resources assessment, during the appropriate season(s) for identification of the resource.

EL-OS-1.8

When reviewing development proposals, consult with the most updated California Natural Diversity Database (CNDDB) and other available studies prepared by the California Department of Fish and Wildlife (CDFW) to avoid potential impacts on sensitive habitats or special-status species.

EL-OS-1.9 *

Require a wetland delineation study prepared using the protocol defined by the US Army Corps of Engineers as part of project approval for proposed development on sites with the potential to contain wetland resources. Require that a report on the findings of this survey be submitted to the County as part of the application process and, where proposed development would impact wetland resources, require mitigation measures prior to project approval.

EL-OS-1.10 *

Require that construction or other ground-disturbing activities avoid nests of native birds when in active use by implementing protection measures to ensure compliance with the California Fish and Game Code and federal Migratory Bird Treaty Act. Compliance guidelines are detailed in the General Plan Environmental Impact Report.

EL-OS-1.11 *

Require installation and maintenance of construction barrier fencing around sensitive resources on or adjacent to construction sites prior to construction activities and throughout the construction period.

EL-OS-1.12

Maintain creeks in their natural state ~~whenever possible~~ and allow creeks and floodways to function as natural flood-protection features during storms.

EL-OS-1.13 *

Require new development along significant watercourses, riparian vegetation, and wetlands to establish an ecological buffer zone between the waterway and development to protect the resource. Consider providing opportunities for multiuse trails and recreation in the buffer zone when appropriate.

EL-OS-1.14 *

Require Creek Management Plans and Mineral Reclamation Plans to include measures to protect and maintain riparian resources and habitats.

EL-OS-1.15

Encourage State and federal agencies that manage public lands in the county to prevent vehicular travel in streambeds to minimize sedimentation and protect overall water quality, and in particular Clear Lake hitch and their habitat.

EL-OS-1.16

Increase planting and regeneration of native trees, shrubs, grasslands, and natural landscapes and protect existing native plant species ~~whenever possible~~ to preserve the visual integrity of the landscape, provide habitat, and improve air quality and community health, especially in Equity Priority Areas.

EL-OS-1.17

Require majority use of California native plant species in large, landscaped areas in new development projects, and require construction practices that avoid spread of invasive plant species by minimizing surface disturbance; seeding and mulching disturbed areas with certified weed-free native mixes; disinfecting and decontaminating equipment; and using native, noninvasive, drought-resistant species in erosion-control plantings.

EL-OS-1.18 *

Protect wildlife migration corridors and, prior to approval of projects that might impact identified wildlife connectivity and wildlife linkage areas, require that a habitat connectivity/wildlife corridor evaluation be completed by a qualified biologist. The evaluation shall identify project design features that would reduce potential impacts to wildlife movement and incorporate wildlife corridors for new development plans where appropriate.

EL-OS-1.19

Discourage use of fencing that poses risks to wildlife.

EL-OS-1.20

Work with conservation agencies to identify appropriate locations and methods for incorporating wildlife crossings into future road projects.

EL-OS-1.21

Encourage Caltrans to consider the installation of wildlife over- or underpasses.

EL-OS-1.22 *

Require lighting in residential areas, and along roadways, and adjacent to sensitive habitat areas to prevent artificial lighting from spilling and/or reflecting into adjacent properties, including natural or open space areas by incorporating design features, such as shielded lighting and landscaping.

EL-OS-1.23

Tribal Ecological Knowledge (TEK) in biological resource management, habitat restoration, and open space stewardship.

Actions**EL-OS-1.a**

Consult with the California Department of Fish and Wildlife to identify significant environments and priorities for acquisition or maintenance of open space areas based on biological and environmental concerns and develop a strategy for maintaining areas that will preserve the populations of plants and animals in the county.

EL-OS-1.b

Prepare and maintain a mapped inventory of ecologically sensitive resource areas, including unique natural areas, wetlands, floodplains, riparian resources, and the habitat of rare, threatened, endangered, and other uncommon and protected species.

EL-OS-1.c *

Adopt a Creek and Riparian Area Management Ordinance in consultation with a qualified biologist to support Clear Lake hitch and other sensitive riparian habitat and educate affected property owners on their responsibilities and options for proper maintenance on private properties. Require projects with the potential to impact creeks and riparian areas to demonstrate compliance with the Ordinance prior to project approval.

EL-OS-1.d

Improve County roads, where needed, to prevent vehicular travel in streambeds to minimize sedimentation and protect overall water quality, and in particular Clear Lake hitch and their habitat.

EL-OS-1.e

Develop educational materials regarding the impacts of off-highway vehicle (OHV) travel in streambeds to sedimentation, water quality, and Clear Lake hitch and their habitat, prioritizing distribution to landowners with riparian areas on their property.

EL-OS-1.f

Develop, adopt, and implement an Oak, Landmark, and Heritage Tree Preservation Ordinance.

EL-OS-1.fEL-OS-1.g

Develop and implement a countywide Wildlife Migration Corridor Preservation Plan.

EL-OS-1.gEL-OS-1.h

Review “WW” Waterway, “FW” Floodway, “FF” Floodway- Fringe, and “W” Wetlands combining district information on a periodic basis. Update the boundaries of these combining districts as needed to reflect updates to mapped ecologically sensitive resource areas.

Goal EL-OS-2

Archaeological, Tribal, cultural, and historical resources that are protected and preserved throughout the Planning Area for the long-term benefit of residents, tourists, and future generations.

Policies

EL-OS-2.1 *

Review proposed development projects in conjunction with the California Historical Resources Information System, Northwest

Information Center, at Sonoma State University, to determine whether project areas contain known archaeological resources, both prehistoric and/or historic-era, and Tribal Cultural Resources (TCRs), or if they have the potential to hold such resources and if so, implement mitigation to protect the resource.

The California Environmental Quality Act (CEQA) Guidelines offer definitions for some of the key terms used in this Goal’s policies and actions and are included in the following pages for reference.

EL-OS-2.2 *

Whenever possible, require new development to preserve significant historical, archaeological, or paleontological resources on-site through project design. If on-site preservation is not feasible, mitigate impacts through techniques such as relocation of structures, adaptive reuse, preservation of facades, and thorough documentation and archival of records. Any documentation of historic resources shall

Tribal Cultural Resources (TCRs) are “sites, features, places, cultural landscapes (geographically defined in terms of the size and scope), sacred places, and objects with cultural value to a California Native American tribe that are included or determined to be eligible for inclusion in the California Register; and/or included in a local register of historical resources; and/or a resource determined by the lead CEQA agency, in its discretion and supported by substantial evidence, to be significant.”

- CEQA Guidelines Section 20174

be conducted in accordance with Historic American Building Survey (HABS) Level III standards, as defined by the US Secretary of the Interior.

EL-OS-2.3 *

Upon discovery of significant archaeological, paleontological, historic, or Tribal Cultural Resources during site development, require all activity to be halted within 100 feet of the find(s) and notify a professional archaeologist, paleontologist, historian, and/or local Tribal government representative (if appropriate) to evaluate the find(s) and recommend mitigation procedures, if necessary. Findings and mitigation measures are subject to review and approval by the Lake County Planning Commission prior to resuming site development work.

A **historical resource** is a resource listed in the California Register of Historical Resources (CRHR) or determined to be eligible for listing in the CRHR by the State Historical Resources Commission, a resource included in a local register of Historical Resources, or identified as significant in a Historical Resource survey meeting the requirements of Public Resources Code (PRC) Section 5024.1(g), or any object, building, structure, site, area, place, record, or manuscript that a lead agency determines to be historically significant or significant in the architectural, engineering, scientific, economic, agricultural, educational, social, political, military, or cultural annals of California.

- CEQA Guidelines Section 15064.5(a)

EL-OS-2.4 *

As a condition of approval for discretionary grading permits, require construction employees to be trained in recognizing potentially significant archaeological, paleontological, historic, or Tribal Cultural Resources that may be discovered during ground-disturbing activities. Require project proponents to submit verification that all site workers have received sensitivity training prior to commencement of ground-disturbing activities.

EL-OS-2.5 *

Require discretionary review for demolition permit applications on important historic sites to mitigate impacts.

EL-OS-2.6

Protect cultural and archaeological sites with potential for placement on the National Register of Historic Places and/or inclusion in the California State Office of Historic Preservation's California Points of Interest and California Register of Historical Resources. Such sites may be of statewide or local significance and have anthropological, cultural, military, political, architectural, economic, scientific, religious, or other values.

A **unique archaeological resource** is "an archaeological artifact, object, or site about which it can be clearly demonstrated that, without merely adding to the current body of knowledge, there is a high probability that it meets any of the following criteria: it contains information needed to answer important scientific research questions and there is a demonstrable public interest in that information; it has a special and particular quality such as being the oldest of its type or the best available example of its type; or it is directly associated with a scientifically recognized important prehistoric or historic event or person."

- CEQA Guidelines Section 21083.2

EL-OS-2.7

Follow the Secretary of the Interior Standards for Preservation, Rehabilitation, Restoration, and Reconstruction for the treatment of historic properties.

EL-OS-2.8

Prioritize preservation and adaptive reuse of buildings, sites, and areas with identified archaeological, cultural, or historic significance.

EL-OS-2.9

Coordinate historic and cultural resources management with local Tribal governments, planning and regulatory agencies, and nearby jurisdictions.

EL-OS-2.10

Support and distribute information to the community from local, State, and national education programs about cultural and archaeological resources.

EL-OS-2.11

Conduct education and outreach to the community about the various federal, State, Local, and private funding sources and economic mechanisms available to support historic resource preservation.

Actions**EL-OS-2.a**

Contract with a qualified cultural resource coordinator for the development of review procedures and ordinances for the protection of cultural heritage resources, including mechanisms for the protection of identified cultural sites from vandalism and instances when owners of designated historic properties assert economic hardship.

EL-OS-2.b *

Contract with an archaeologist(s) that meets the Secretary of the Interior's Professionally Qualified Standards to perform cultural resource studies for environmental analysis on discretionary projects where a cultural resource report is deemed necessary, including outreach to Tribal governments for input on Tribal Cultural Resources.

EL-OS-2.c

Establish a process for considering the designation of structures older than 50 years as historic structures or landmarks eligible for special recognition and preservation protections.

EL-OS-2.d

Support an ongoing Lake County Historic Commission to review development projects that may impact historic resources.

EL-OS-2.e

Engage the East Lake community to identify appropriate sites for the Historic Preservation Combining District and apply the District to identified sites.

EL-OS-2.f

Support continued operation of the Lake County Heritage Commission to review development projects that may impact historic resources.

EL-OS-2.g

Apply to the State to become a Certified Local Government, making the County eligible for historic preservation programs.

Goal EL-OS-3

Celebrated, amplified, and preserved tribal influences and Tribal Cultural Resources throughout the East Lake Communities Planning Area.

Policies**EL-OS-3.1**

Protect tribal heritage, honor the early stewards of this land, and treat Native American remains and Tribal Cultural Resources with sensitivity and respect.

EL-OS-3.2

Support educational and other opportunities to increase knowledge and promote awareness of the history of indigenous peoples in what is now the East Lake Communities Planning Area, and how the County's relationship with local tribes has evolved.

EL-OS-3.3

Respect and protect Tribal Cultural Resources, including historic, cultural, and sacred sites; cultural landscapes; views of or access to resources; and objects with cultural value to California tribes, recognizing that protecting Tribal Cultural Resources supports both cultural preservation and environmental health outcomes.

EL-OS-3.4

Conduct formal and early government-to-government consultation with culturally affiliated Tribes on all projects with potential cultural, environmental, or scenic impacts to identify and protect recorded and unrecorded Tribal Cultural Resources through the discretionary development review process.

EL-OS-3.5

Require protection of cultural resource sites in subdivision projects by avoiding identification of sites on maps, relying instead on other techniques, including establishing building envelopes.

EL-OS-3.6 *

Consult with culturally affiliated Tribal governments when developing mitigation measures to avoid or minimize impacts on Tribal Cultural Resources. Mitigation could include notification of culturally affiliated Tribal governments prior to and tribal monitoring during ground-disturbing activities.

EL-OS-3.7 *

Upon discovery of a burial, human remains, or suspected human remains, require immediate halt to all work within 100 feet and notify the Sheriff's Department, the culturally affiliated Tribal governments, and a qualified archaeologist for proper internment and tribal rituals according to Public Resources Code Section 5097.98 and Health and Safety Code Section 7050.5.

EL-OS-3.8

Work with appropriate Tribal governments to protect sacred sites from visual intrusion or development inconsistent with cultural values, particularly along scenic corridors and riparian areas.

Actions**EL-OS-3.a**

Establish and maintain collaborative relationships with local Tribal government representatives to facilitate tribal consultation, preserve Tribal Cultural Resources, and coordinate land use planning in areas around tribal lands.

EL-OS-3.b

Work with local Tribal governments to establish programs and secure funding to implement actions aimed at preserving Tribal Cultural Resources.

EL-OS-3.c

Collaborate with Tribal governments to identify cultural sites, sacred landscapes, cultural viewsheds, and traditional use areas, and incorporate them into land use planning through a Tribal Resource Conservation Overlay or equivalent mechanism, that requires a consultation-based review for any future land use designation changes affecting these areas to protect Tribal Cultural Resources and uphold the tribes' ongoing stewardship of their ancestral lands. Where appropriate, explore partnerships for co-management, conservation, and interpretive uses that honor the ancestral and ongoing relationship between Tribal governments and these lands.

Goal EL-OS-4

Conserved energy resources.

Policies**EL-OS-4.1**

Require the use of energy conservation features and encourage renewable and carbon-free energy generation and storage systems in new construction and renovation of existing structures.

EL-OS-4.2

Support efforts to increase renewable and carbon-free energy generation, including wind, solar, and biomass.

EL-OS-4.3

Promote and incentivize small-scale, on-site renewable energy and storage systems for existing residential units, nonresidential buildings, and in the agricultural sector.

EL-OS-4.4

Participate in local and State programs that reduce the consumption of energy.

EL-OS-4.5

Work with energy providers to promote and expand cost-saving options for low-income households.

EL-OS-4.6

Construct all new or remodeled County buildings and retrofit existing buildings to be energy efficient and rely on renewable and carbon-free energy resources where feasible.

EL-OS-4.7

Consider fuel efficiency as one of the main selection criteria when purchasing new County vehicles.

EL-OS-4.8

Coordinate with energy providers to place transmission systems in locations that minimize environmental and other impacts.

Actions**EL-OS-4.a**

Prepare a new General Plan Energy Element to guide the development, governance, financing, and ongoing management of energy generation projects and energy resources within the county in a manner that maximizes benefits to the Lake County community.

EL-OS-4.b

Investigate use of community benefit agreements for new energy generation developments.

EL-OS-4.aEL-OS-4.c

Explore opportunities to purchase and install solar panels and battery storage in bulk to serve homes and businesses in the community, prioritizing Equity Priority Areas and Equity Priority Populations, including establishment and subsidization of community solar programs that supply electricity to multiple consumers from a single photovoltaic solar location.

EL-OS-4.bEL-OS-4.d

Explore options to create a bulk buying program or revolving loan fund that allows people to purchase energy-efficient electric appliances (air conditioners, fans, air purifiers, heaters, stoves, ovens, etc.) and other items that increase building efficiency at wholesale prices or minimal cost to increase energy efficiency and replace hazardous appliances in their home or business. Work with volunteer groups to support installation and disposal of old appliances in Equity Priority Areas for Equity Priority Populations.

EL-OS-4.cEL-OS-4.e

Coordinate with local utility providers and educational institutions to develop informational brochures that promote energy conservation and the use of renewable and carbon-free energy sources. Make these brochures available to the public and distribute them with building permit application materials.

EL-OS-4.dEL-OS-4.f

Work with local agencies and non-profits to retrofit and upgrade homes for energy efficiency and climate resiliency, particularly in Equity Priority Areas.

Goal EL-OS-5

Continued economic viability of mineral extraction activities while minimizing impacts on the public and the environment.

Policies

EL-OS-5.1

Require that all mining operations prevent environmental contamination from storage and disposal of wastes and hazardous materials or general operating activity at the site.

EL-OS-5.2

Ensure that all mining operations are performed in compliance with the policies and implementation measures included in the 1992 Lake County Aggregate Resources Management Plan (**Appendix A** to this General Plan).

EL-OS-5.3

Require new development adjacent to existing mining operations to be designed to provide a buffer between the proposed use and the mining operation. Require new mining operations to provide a buffer between the mine and existing or likely adjacent uses to minimize incompatibility issues and mitigate environmental and aesthetic impacts. Required buffer distances will be based on an evaluation of noise, aesthetics, drainage, operating conditions, topography, lighting, traffic, operating hours, and air quality.

EL-OS-5.4

Promote the use of Best-Available Control Technology to minimize dust, smoke, fumes, and odors associated with mining and processing operations.

EL-OS-5.5

Require review and approval by the Environmental Health Department and Air Quality Management District for storage of hazardous materials associated with new mining operations.

Goal EL-OS-6

A parks, recreation, and open space system that serves the recreational needs of the East Lake Communities Area residents and visitors.

Policies

EL-OS-6.1

Provide and promote a balanced recreation system offering a variety of quality social activities and recreational opportunities for all community members.

EL-OS-6.2

Coordinate the development of parks and recreation facilities with the rate of growth in the East Lake Communities Planning Area to ensure parks and recreation facilities are adequate to serve the local population.

EL-OS-6.3

Maintain a level of service standard of five acres per 1,000 residents for parks.

EL-OS-6.4

Support the continued maintenance and improvement of existing recreational facilities and expansion of new recreational opportunities on County-owned lands, as outlined in the countywide Parks, Recreation, and Trails Master Plan, and on State- and federally-owned lands.

EL-OS-6.5

When constructing new County parks, use the following guidelines:

- Locate the park to promote an equitable distribution of facilities within the Planning Area and across the county.
- Locate the park centrally to serve as much of the population base as possible.
- Locate the park at the intersection of major and/or minor collectors to facilitate access.
- Locate the park along designated bicycle routes to facilitate access by bicyclists.
- Locate the park in close proximity to shopping areas, cultural facilities, such as libraries or museums, schools, and employment centers.
- Develop community parks to be appropriately sized.
- Provide night lighting when feasible to reduce vandalism and extend the use of the facility.
- Locate the park on sites that do not contain Naturally Occurring Asbestos, unless adequate clean cover material and other effective controls, as determined by the Lake County Air Quality Management District, are incorporated to minimize public exposure to asbestos dust.

EL-OS-6.6

Expand access to diverse, high-quality parks, green spaces, recreational facilities, trails, passive recreation, and natural environments for Equity Priority Populations and residents of Equity Priority Areas, including by facilitating multiple transportation modes and installing amenities such as paved

paths, shade trees, and benches to existing facilities. Design and maintain these facilities to offer a safe and comfortable environment for users of all ages and abilities.

EL-OS-6.7

Strengthen the role of County parks as community focal points by constructing meeting halls and recreational buildings on existing parks where feasible.

EL-OS-6.8

Prioritize and promote recreational activity programs and opportunities in Equity Priority Areas.

EL-OS-6.9

Support expanded access to indoor and outdoor recreation opportunities by working with other agencies to co-locate recreation spaces, parks, and trails with public facilities, such as schools and utility easements, prioritizing Equity Priority Areas.

EL-OS-6.10

Require large development projects to construct and maintain parks within the development, when appropriate.

EL-OS-6.11

Support partnerships with local Tribal governments for joint management and interpretation of parks, trails, and open space areas to reflect and honor the tribes' enduring connection to the land.

EL-OS-6.12

Involve community members in the design and development of all park and recreation facilities, with special emphasis on engaging Equity Priority Populations and residents of Equity Priority Areas.

EL-OS-6.13

When building parks and recreational facilities in areas with sensitive habitat, preserve habitat areas and design the space to avoid potential impacts on the habitat from park users.

EL-OS-6.14

Promote passive recreation in flood-prone areas to absorb floodwater and protect developed areas.

EL-OS-6.15

Seek opportunities to install bird watching amenities in the County parks.

Actions

EL-OS-6.a

Implement and periodically update, as needed, the countywide Parks, Recreation, and Trails Master Plan.

EL-OS-6.b

Seek funding to implement the Parks, Recreation, and Trails Master Plan and to keep parks safe, attractive, and responsive to community needs, including funding for park acquisition, planning, capital improvements, lifecycle replacement of amenities, recreation programming, recreation centers, and maintenance operations.

EL-OS-6.c

Identify public lands suitable for off-road recreation and explore opportunities to develop these uses.

EL-OS-6.d

Investigate the feasibility of developing a park expansion and improvement program that includes a combination of one or more of the following: impact fees for new homes, special assessment districts, and county service areas.

EL-OS-6.e

Work to ensure parks and recreation spaces are kept clean and free of litter and hazardous materials.

EL-OS-6.f

Enforce prohibitions of drug use and overnight and/or long-term camping in parks, recreation, and open space areas.

Goal EL-OS-7

Public access to and opportunities for community enjoyment of Clear Lake.

Policies**EL-OS-7.1**

Provide trails, turnouts, and public open spaces wherever possible along the lakeshore.

EL-OS-7.2

Manage County-owned land near Clear Lake in a manner that maximizes public enjoyment of the lake, including by:

- Retaining vacant County-owned lakefront properties and making improvements, as needed, to expand opportunities for public access to and use of Clear Lake, unless a land swap, as described below, would result in better public access;
- Considering land swaps with private lakefront property owners that would enable consolidation of County properties into more useable parcels;
- Acquiring surplus lakefront lands from other public agencies and retaining them for public lake access;
- Prioritizing expansion of lakefront lands for public access adjacent to existing lakefront recreational lands to foster an integrated lakefront recreation network; and
- Utilizing public streets that dead-end at the lake for public access to the lake.

EL-OS-7.3

Continue to support efforts to improve and increase public access to Clear Lake, prioritizing sites in established communities and near existing resort development.

EL-OS-7.4

Require new commercial development along the shoreline of Clear Lake to provide public lake access, including docks, as appropriate.

EL-OS-7.5

Support water-related uses and facilities on vacant or redeveloped lakefront lands, provided the uses and/or facilities will not negatively impact the environment or preclude public access to the lake.

Water-related uses and facilities are those whose primary purpose is to provide for public access to and use of the lake, such as fishing, boating, swimming, water skiing, viewing, and commercial uses that derive major economic benefits from their immediate proximity to the lake.

EL-OS-7.6

Discourage conversion of privately owned lakefront lots that provide public access to the lake to uses that would preclude future public use.

EL-OS-7.7

Prioritize maintenance and improvement of public lakefront lands in the County's budget to help maximize the recreation, aesthetic, and economic benefits derived from public view and use of the lakefront.

EL-OS-7.8

Maintain a safe boating environment on the County's waterways.

EL-OS-7.9

Maintain and expand County boat launch facilities to ensure adequate and convenient access to Clear Lake.

Actions

EL-OS-7.a

Continue to maintain the Clear Lake public launch and marina facilities to ensure easy and safe access by the public.

EL-OS-7.b

Investigate development of dock maintenance regulations.

EL-OS-7.bEL-OS-7.c

Install and maintain signage identifying areas open to public access along the lake.

EL-OS-7.cEL-OS-7.d

Explore innovative programs that would expand public lake access in residential areas along the lake.

EL-OS-7.dEL-OS-7.e

Explore the feasibility of developing additional boat launch facilities at streets that dead-end at the lake.

EL-OS-7.eEL-OS-7.f

Establish temporary docking areas for boats used in commuting across Clear Lake in locations where the County maintains dock facilities.

EL-OS-7.fEL-OS-7.g

Establish no-wake zones in ecologically sensitive resource areas and areas primarily used for nonmotorized recreation, such as swimming, kayaking, and bird watching.

Goal EL-OS-8

An interconnected multiuse trail system that serves a variety of recreational needs in the East Lake Communities Planning Area.

Policies

EL-OS-8.1

Encourage and support efforts to develop a multiuse ridgeline trail system and multiuse shoreline water trail system along the north shore of Clear Lake.

Infrastructure to support walking and biking, including trails, is also addressed in the Circulation Element.

EL-OS-8.2

Encourage landowners to create easements along waterways or across lands that would connect existing and proposed trails.

EL-OS-8.3

Provide safe access to existing and new parks for hikers, cyclists, and equestrians. Support development of improved and additional safe access for hikers, cyclists, and equestrians on State and federal lands.

EL-OS-8.4

Continue to support equestrian activities in the East Lake Communities Planning Area, including through land use regulations that support horse ownership and stewardship.

EL-OS-8.5

Increase public awareness and support for county trails through promotions, interpretive programs, exhibits, publication, maps, and the County's website.

EL-OS-8.6

Provide water and shade structures at trail heads and county parks, where feasible, to support their use and decrease adverse health impacts from recreation during extreme heat days.

Actions

EL-OS-8.a

Develop incentives to encourage dedication of trails.

EL-OS-8.b

Develop trails consistent with the County Bikeway Plan and the Konocti Regional Trails Plan for hiking, bicycling, and riding.

EL-OS-8.c

Develop nature and water trails along the shoreline, including to provide access for non-motorized boating.

Goal EL-OS-9

Preserved and enhanced scenic resources and viewsheds in the East Lake Communities Planning Area.

Policies

EL-OS-9.1 *

In rural areas outside of Community Growth Boundaries, require structures built in the immediate foreground view of a scenic route to be sited a sufficient distance from the roadway edge to minimize intrusion on the natural features and backdrops as viewed from the roadway or adjacent residences. In rural and urbanized areas, require structures built in the immediate foreground of a scenic route to minimize obstruction of views of significant natural features. Require development on hillsides to blend in with the surroundings through exterior color choices and maximum height restrictions.

EL-OS-9.2

Develop and maintain roads and highways in a manner that protects natural and scenic resources.

EL-OS-9.3

Develop access to scenic viewpoints along County roadways and multiuse trails. Provide interpretive information at these access points to inform visitors and residents of the natural and cultural history of the East Lake Communities Planning Area and region.

EL-OS-9.4

Encourage utility companies and service providers to choose the least conspicuous locations for distribution lines, towers, and fuel breaks to avoid impacts to viewsheds where possible.

EL-OS-9.5

Use native and drought tolerant landscaping to frame and direct attention to major views and away from unattractive developments to the extent practical. Permit selective cutting and pruning to enable establishment or improvement of roadway views. Discourage the use of herbicides wherever possible.

EL-OS-9.6

Ensure light does not impact the dark skies of Lake County.

Actions

EL-OS-9.a *

Develop an ordinance that regulates development on or near ridgelines, hilltops, and along the shoreline of Clear Lake to minimize impacts to scenic viewsheds.

EL-OS-9.b

Amend the Scenic Combining District to include ridgelines, working in consultation with Tribal governments.

EL-OS-9.c

Amend the Scenic Combining District regulations in the Zoning Ordinance to prohibit billboard signage on commercial properties.

EL-OS-9.d

Work with local Tribal governments to establish a sacred viewshed preservation policy, balanced with economic development and private property rights.

Goal EL-OS-10

Preserved and enhanced visual quality in community areas.

Policies

EL-OS-10.1

Require new development to screen mechanical equipment and trash storage facilities from public view.

EL-OS-10.2

Require appropriate visual screening and roadway setbacks for industrial and service commercial uses, including through the use of native vegetation for screening.

EL-OS-10.3 *

Require the use of lighting that enhances visibility, convenience, and public safety without creating glare or light pollution.

EL-OS-10.4 *

Require the use of low-glare building materials for new buildings constructed in the Planning Area.

EL-OS-10.5

Require new commercial development to install street trees in accordance with an adopted streetscape plan and with fire-safe landscaping requirements.

EL-OS-10.6

Require new utilities to be installed underground whenever feasible.

EL-OS-10.7

Require removal of dilapidated structures and signs on the Clear Lake shoreline, including docks, following the abatement procedures dictated by the Uniform Abatement of Public Nuisances Code.

Actions**EL-OS-10.a ***

Amend the Zoning Ordinance to regulate outdoor lighting in a way that minimizes light pollution that disrupts dark skies while providing for safety and reasonable use.

EL-OS-10.b

Amend the Zoning Ordinance to establish additional standards to preserve public views from both the land and the lake, applicable to new residential development and replacement of existing structures with larger ones on the shoreline of Clear Lake.

EL-OS-10.c

Establish a coordinated roadway signage program for the East Lake Communities Planning Area that identifies routes and major destinations, provides traffic information, and provides directional information to major destinations and public access areas while supporting the visual character of the region.

EL-OS-10.d

Work with PG&E to establish an underground utility district to bury existing utilities.

EL-OS-10.e

Expand enforcement of the Zoning Ordinance and Uniform Abatement of Public Nuisances Code to abate signs, billboards, and shoreline protection structures, such as seawalls and retaining walls, that do not meet Code requirements.

EL-OS-10.f

Maintain Code Enforcement services to support the visual quality of the community.

Environmental Justice

In 2016, the California State legislature passed SB 1000, making the topic of environmental justice a required element in General Plans. This new Environmental Justice Element for the East Lake Communities Planning Area accomplishes the following:

- Introduces the concept of environmental justice.
- Summarizes the process used to identify communities in the East Lake Communities Planning Area that are impacted by environmental justice issues (termed Equity Priority Areas).
- Identifies environmental justice issues in the East Lake Communities Planning Area, which include pollution exposure, access to public facilities and services, food security, availability of healthy living spaces, abandoned properties and illegal dumping issues, access to healthcare, access to recreation, and opportunities for community engagement.
- Establishes goals, policies, and actions to address these issues and the needs of impacted communities. As noted in the Introduction, because environmental justice touches all aspects of the community, some of these policies and actions are in other East Lake Communities Planning Area Elements.

The concept of environmental justice recognizes that low-income residents, communities of color, Indigenous peoples, and tribal nations are disproportionately exposed to environmental health hazards like soil and water contamination and that this inequity is the result of many factors that have occurred throughout history and are still occurring today. Some of these factors include inappropriate zoning and negligent land use planning, intersecting structural inequalities, deed restrictions and other discriminatory housing and lending practices, limited political and economic power among these demographic groups, the prioritization of business interests over public health, development patterns that tend to concentrate pollution and environmental hazards in these communities, and the placement of economic and environmental benefits in other communities. As a result, these impacted communities continue to face significant barriers to their overall health, livelihood, and sustainability.

Environmental Justice Definition

Environmental justice is defined as “the fair treatment and meaningful involvement of people of all races, cultures, incomes, and national origins with respect to the development, adoption, implementation, and enforcement of environmental laws, regulations, and policies...and includes, but is not limited to, all of the following:

The availability of a healthy environment for all people.

The deterrence, reduction, and elimination of pollution burdens for populations and communities experiencing the adverse effects of that pollution, so that the effects of the pollution are not disproportionately borne by those populations and communities.

Governmental entities engaging and providing technical assistance to populations and communities most impacted by pollution to promote their meaningful participation in all phases of the environmental and land use decision making process.

At a minimum, the meaningful consideration of recommendations from populations and communities most impacted by pollution into environmental and land use decisions.”

- *California Government Code Section 65040.12(e)(1).*

Equity Priority Areas and Populations

Among the requirements to comply with SB 1000, jurisdictions must identify environmental justice-impacted communities; in Lake County, these are termed Equity Priority Areas. State law allows jurisdictions to use discretion in identifying Equity Priority Areas, though the State provides several possible thresholds and data sources to inform the designation. Guidance from the California Environmental Protection Agency (CalEPA) suggests including federally recognized tribal lands among the Equity Priority Areas, in addition to areas that have certain social characteristics, such as a concentration of low-income households, and that are disproportionately affected by environmental pollution and other hazards. The inclusion of tribal areas in this definition reflects an understanding of the ongoing and generational impacts of colonization, displacement, and marginalization affecting indigenous peoples today, evidenced by disproportionately negative economic and public health outcomes in tribal communities.

Following this guidance, the Elem Indian Colony of Pomo Indians of the Sulphur Bank Rancheria, situated along the shore of Clear Lake between Clearlake Oaks and Clearlake, is identified as an Equity Priority Area in this element.

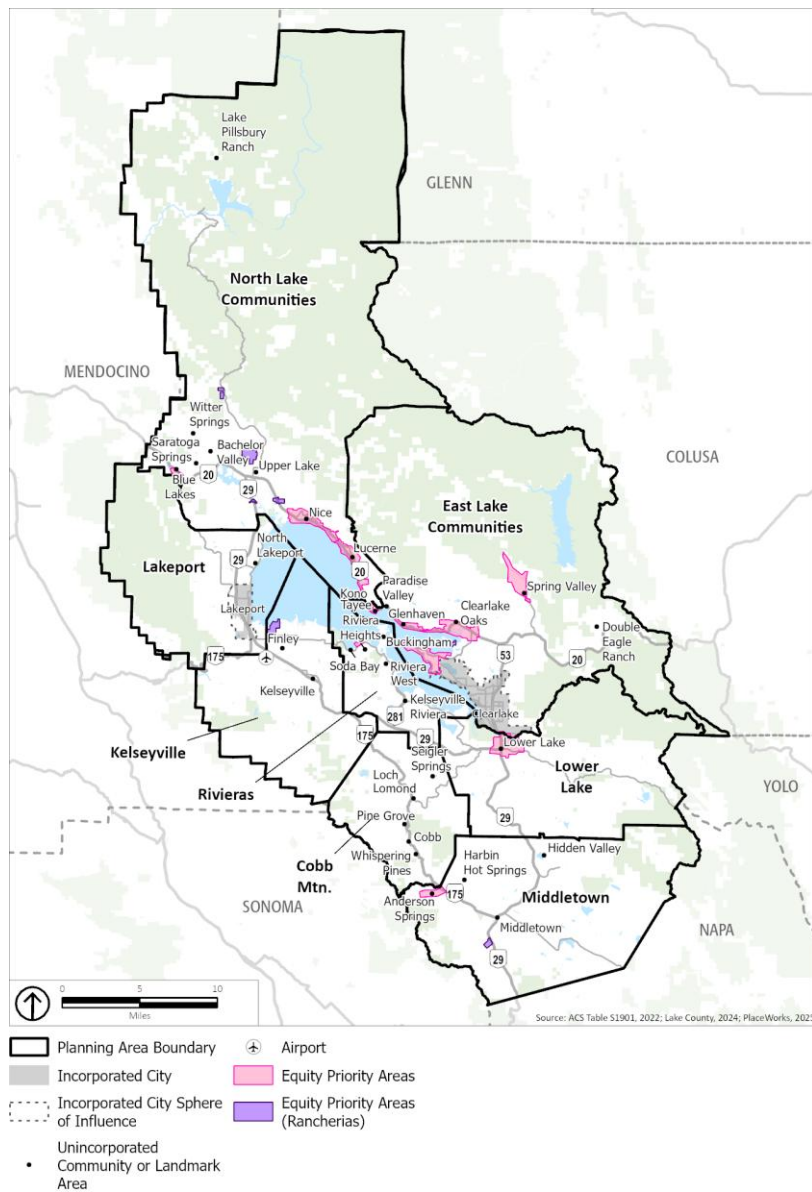
The County also consulted income data to identify the most economically disadvantaged areas as Equity Priority Areas. Specifically, the County identified census block groups where the median income is 80 percent or less of the countywide area median income. These areas were then trimmed to only include populated areas and exclude open spaces. Based on this approach, most of the populated areas in the East Lake Communities Planning Area are considered Equity Priority Areas, including Glenhaven, Kono Tayee, Clearlake Oaks, Spring Valley, and the Gooseneck area, which is a peninsula south of Clearlake Oaks and west of Borax Lake that extends into Clear Lake.

Tribal and income-based Equity Priority Areas are shown in **Figures EL-12** (countywide) and **EL-13** (East Lake Communities Planning Area).

In addition to these designated Equity Priority Areas, the County recognizes that there are community members throughout the county who may be acutely vulnerable to being underserved in a variety of areas, including environmental justice, health, and safety. The General Plan terms these Equity Priority Populations, which include the following groups of people:

- Older adults
- Individuals with disabilities
- Farmworkers and other outdoor workers
- Non-English speakers
- People of color and immigrant communities
- Tribal communities
- Low-income individuals and households
- Unhoused people

Figure EL-12 Countywide Equity Priority Areas



Policies and actions in this General Plan to address environmental justice issues that are place-specific, such as the clean-up of abandoned lots, may apply to the entire Planning Area or county but include language to first prioritize addressing the needs of Equity Priority Areas. Similarly, policies and actions that are not necessarily tied to a geographic location but do address an environmental justice issue may include references to Equity Priority Populations. Some policies and actions focus on both Equity Priority Areas and Equity Priority Populations.

Environmental Justice Issues in the East Lake Communities Planning Area

This section describes environmental justice issues identified in the East Lake Communities Planning Area based on State data and conversations with community members, including tribal communities.

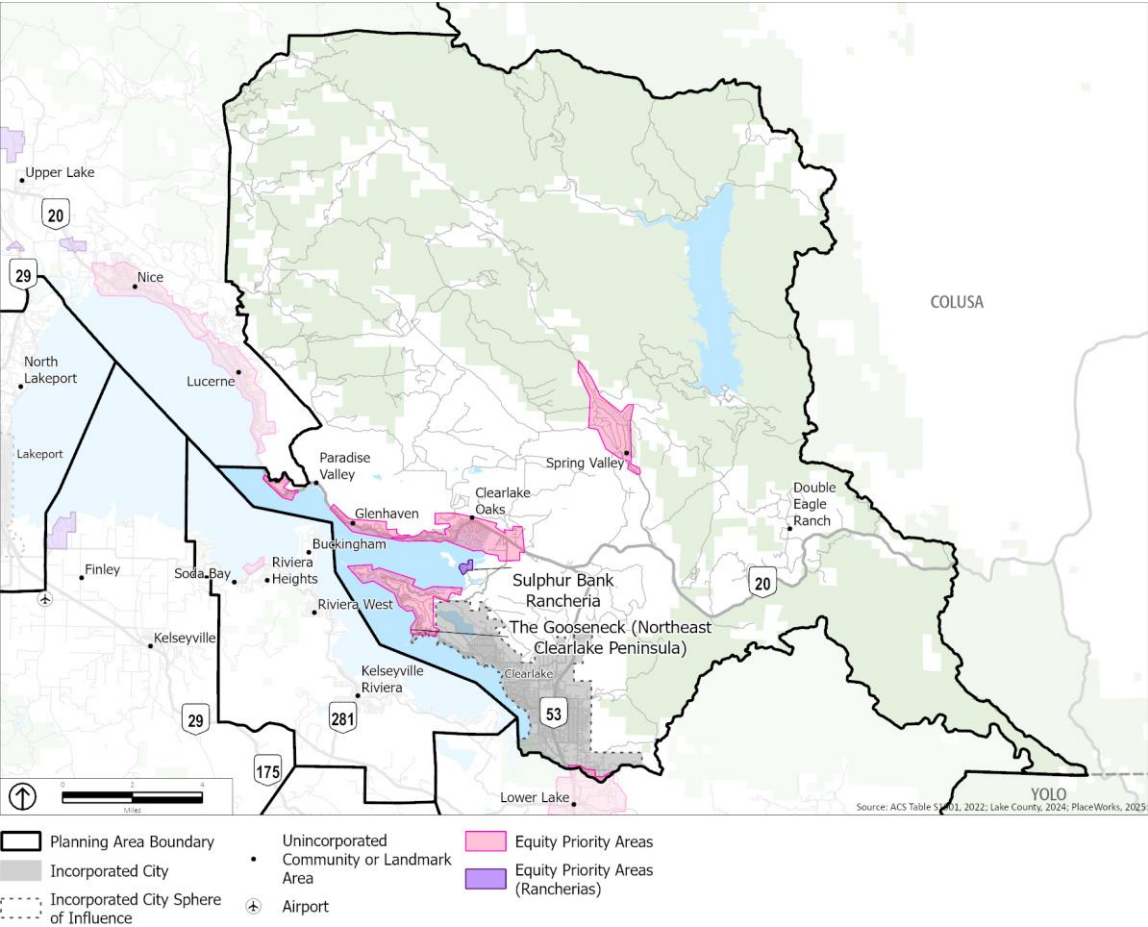
State Data

The Office of Environmental Health Hazard Assessment (OEHHA), which is part of CalEPA, developed and maintains a tool called CalEnviroScreen, which measures pollution and population characteristics using 21 indicators related to environmental justice (see **Table EL-5**). CalEnviroScreen ranks each census tract in the state for each indicator, with higher scores indicating the census tract fares worse for that issue than census tracts with lower scores. For example, a ranked score of 75 indicates that the census tract is more impacted for that indicator compared to 75 percent of all other census tracts in the state. Higher scores always indicate a higher burden or negative impact. **Figure EL-14** shows the census tracts encompassing the populated East Lake Communities Planning Area used to evaluate environmental justice-related data from CalEnviroScreen.



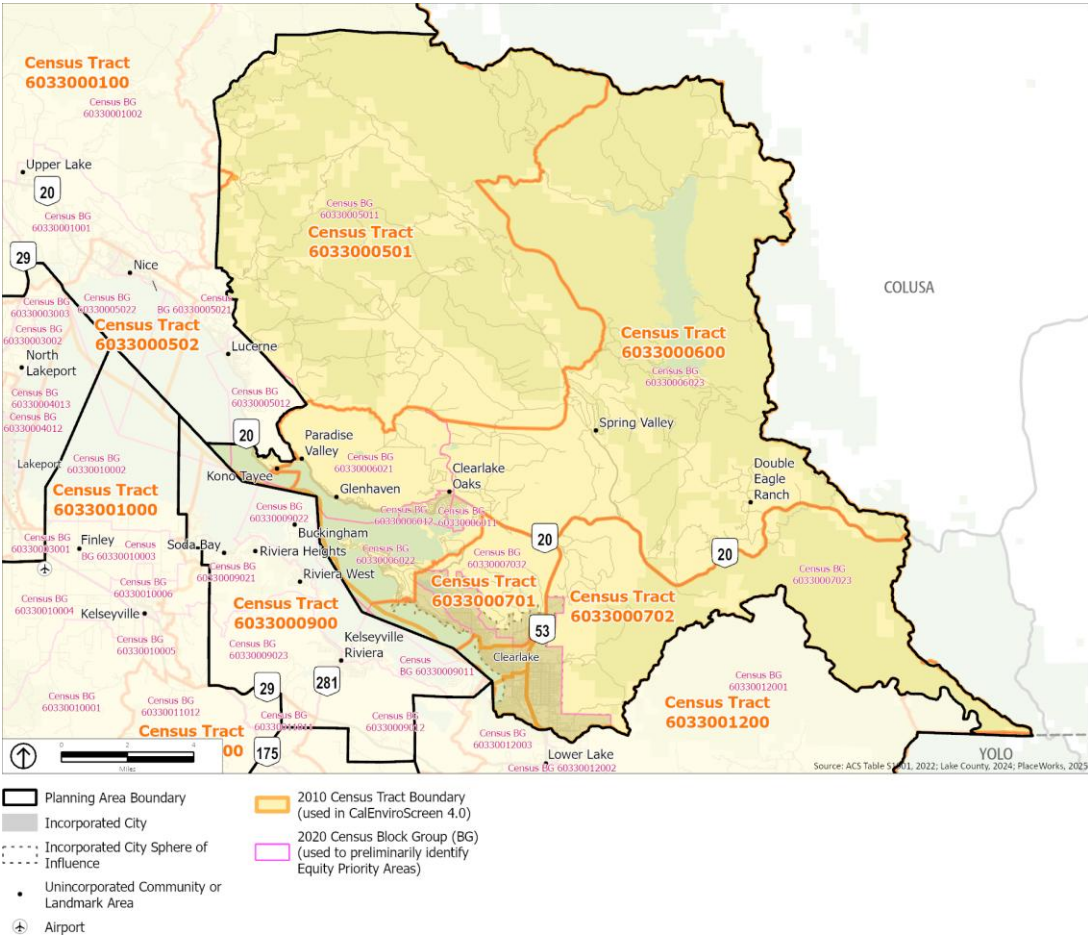
Low-income communities, communities of color, and tribal communities are disproportionately exposed to environmental health hazards like environmental clean-up sites.

Figure EL-13 East Lake Communities Planning Area Equity Priority Areas



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Figure EL-14 East Lake Communities Planning Area Census Tracts and Block Groups



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Table EL-5 CalEnviroScreen Scores for East Lake Communities Planning Area

Indicator	Census Tract 6033000501 Score (higher scores indicate higher impact or burden)	Census Tract 6033000600 Score (higher scores indicate higher impact or burden)	Census Tract 6033000701 Score (higher scores indicate higher impact or burden)
Population (2019)	3,047	3,955	4,171
Median Income (2019)	\$42,101	\$46,512	\$29,052
Cumulative CalEnviroScreen Percentile Score (all indicators combined)	30	38	44
Air Quality: Ozone Concentration	15	18	17
Air Quality: Particulate Matter Concentration	0	0	0
Diesel Particulate Matter Emissions	3	4	5
Pesticide Use	26	44	40
Concentration of Toxic Releases	1	1	1
Traffic Density	0	3	4
Concentration of Drinking Water Contaminants	25	78	66
Children's Lead Risk from Housing	55	38	22
Concentration of Cleanup Sites	69	69	69
Susceptibility to Groundwater Threats	0	7	0
Density of Hazardous Waste Facilities	0	0	0
Proximity to Impaired Water Bodies	24	51	24
Proximity to Solid Waste Sites	53	53	81
Asthma-Induced Emergency Room Visits	82	89	96
Low Birth Weight Infants	96	31	97
Heart Attack-Induced Emergency Room Visits	57	68	77
Population Without a High School Degree	67	46	61
Limited English-Speaking Households	33	10	17
Poverty	75	70	88
Relative Unemployment*	20	76	89
Housing Burdened Low-Income Households	78	60	36

Source: CalEnviroScreen 4.0; California Office of Environmental Health Hazard Assessment, updated 2021;
[American Community Survey, Tables S1901 and DP05, 2019 5-Year Estimates](#).

As shown in **Table EL-5**, census tract 6033000501, which encompasses part of the Spring Valley Equity Priority Area, scores above the 50th percentile for 9 of the 21 indicators; its worst burdens are poverty (75th percentile), housing burdened low-income households (78th percentile), asthma-induced emergency visits (82nd percentile), and low birth weight infants (96th percentile). Census tract 6033000600, which encompasses the remainder of Spring Valley and all other Equity Priority Areas in this Planning Area, scores above the 50th percentile for 8 of the 21 indicators, with the following worst burdens: concentration of drinking water contaminants (78th percentile), relative unemployment (76th percentile), and asthma-induced emergency room visits (89th percentile). Census tract 6033000701, which encompasses a small portion of the Clearlake Oaks Equity Priority Area, scores above the 50th percentile for 9 of the 21 indicators; its worst burdens are heart attack-induced emergency room visits (77th percentile), proximity to solid waste sites (81st percentile), poverty (88th percentile), relative unemployment (89th percentile), asthma-induced emergency room visits (96th percentile), and low birth weight infants (97th percentile). The following sections describe the indicators that score above 75 percent in more detail.

Environmental Indicators: Drinking Water Contaminants, Proximity to Solid Waste Sites

The CalEnviroScreen drinking water contaminants score is derived from annual water quality reports published by public water providers. The indicator score combines information about 14 contaminants and three types of water quality violations that can occur during routine testing of public drinking water samples. Specifically, the CalEnviroScreen score calculates the average concentrations for these contaminants in each water system in the state. Both natural and human sources can contaminate drinking water. Natural sources can be found in rocks and soil or come from fires. Human sources include factories, sewage, and runoff from farms. Low-income communities and people in rural areas are exposed to contaminants in their drinking water more often than people in other parts of the state. In the East Lake Communities Planning Area, the census tract ending in 600, which encompasses the majority of the Equity Priority Areas and contains the Sulphur Bank Mine, scores in the 78th percentile for drinking water contaminants, potentially related to impacts from historic operation of the mine.

The CalEnviroScreen proximity to solid waste score is measured based on the proximity of residential households to facilities that process solid waste, like dump sites, recycling facilities, and composting facilities. The census tract ending in 701, which encompasses a small portion of the Clearlake Oaks Equity Priority Area, scores in the 81st percentile.

Health Outcome Indicators: Asthma, Low Birth Weight Infants, Heart Disease

In CalEnviroScreen, rates of asthma and heart disease are measured by the number of emergency department visits between 2015 and 2017 for asthma attacks or heart attacks, respectively. Similarly, low birthweight is measured by the rate of infants born with low birth weights in the same period. Poor health outcomes like asthma, heart disease, and infants born with low birth weights are very closely linked with high poverty rates; low-income households are less likely to be able to physically access and afford high-quality, healthy foods, health care, particularly pre-natal and pediatric healthcare, and have more limited opportunities for outdoor recreation and active transportation like walking and biking. Asthma rates are also linked to the prevalence of toxic building materials common in older housing units, like mold and asbestos, as well high rates of pesticide use from agricultural activities. All three census tracts score

above 75 percent for the asthma indicator. For heart disease, only the census tract ending in 701, which encompasses a small portion of the Clearlake Oaks Equity Priority Area, scores above 75 percent. The census tracts ending in 501 and 701 (portions of the Spring Valley and Clearlake Oaks Equity Priority Areas) score above 75 percent.

Socioeconomic Indicators: Housing Burden, Relative Unemployment, Poverty

Housing burden refers to the percentage of total household income spent on housing costs. This indicator measures the percentage of low-income households (i.e., households making less than 80 percent of the county family median income) that are severely burdened by housing costs (i.e., paying over 50 percent of their income on rent or mortgage). This is a particular issue for residents in the census tract ending in 501, which encompasses a portion of the Spring Valley community, meaning that residents there have limited income remaining after paying their rent or mortgage to pay for other basic needs, like food and health care. This can lead to poor health outcomes and housing instability.

CalEnviroScreen measures poverty based on U.S. Census data and the Federal Poverty Line income threshold, which is calculated regularly based on inflation and is used to qualify households for federal assistance programs. According to CalEnviroScreen, there are more residents in each of the three census tracts encompassing the East Lake Communities Equity Priority Areas living at or below the Federal Poverty Line than at least 70 percent of all other census tracts in California. Low-income households face extreme challenges due to the gap between household income and rising costs of living, particularly in California where both income and sales taxes are relatively high compared to other states.

The unemployment indicator is based on U.S. Census data that measures the percentage of the population over the age of 16 that is unemployed and eligible for the labor force. This excludes retirees, students, military personnel on active duty, and those not looking for work. Unemployment is highly correlated with generational poverty and has a wide range of effects on physical and mental health that can further compound other burdens faced by vulnerable communities. This is a particular issue in the census tracts ending in 600 and 701, which encompass at least portions of all Equity Priority Areas in this Planning Area.

Community-Identified Issues

The CalEnviroScreen indicators provide a general high-level snapshot of potential environmental justice issues in the area, but the data must be “ground-truthed” or used in concert with community engagement to develop a complete understanding of environmental justice and public health issues specific to each community. Therefore, this section summarizes public health and environmental justice issues raised by community members and public health advocates during the General Plan 2050 engagement process. It also describes environmental issues faced by tribal communities based on tribal consultation.

Pollution Exposure

Air quality is very good overall in Lake County and in the East Lake Communities Planning Area, as detailed by the related CalEnviroScreen indicators in **Table EL-5**. However, the Planning Area occasionally experiences poor air dispersion, leading to temporarily poorer air quality, due to its topography. Steep hills, ridges, valleys, and canyons in the Clear Lake Basin can prevent dispersion of pollutants. This effect is especially pronounced during inversion periods, when cold air is trapped near the ground beneath a layer of warm air, preventing contaminated air from rising and being carried away, instead causing it to be spread horizontally. Smoke is a frequent air quality hazard in the Planning Area and throughout Lake County. Sources of outdoor air pollution from smoke include residential burning, prescribed vegetation burning, agricultural burning, and wildfires.

While air pollution is not an issue exclusive to the identified Equity Priority Areas, Equity Priority Populations may be more vulnerable to health impacts from poor air quality due to preexisting health conditions, excess exposure (e.g., outdoor farmworkers), limited incomes, and other constraints, like access to quality healthcare. Policies and actions addressing air quality are included in the Health and Safety Element.

Water pollution from agricultural activity, including illegal cannabis cultivation, as well as from high concentrations of septic tank usage, creek stabilization activities, and wetlands loss and degradation, is also a concern across the county, including in the East Lake Communities Planning Area. The Water Resources and Open Space, Conservation, and Recreation Elements discuss these issues in more detail and provide policies and actions to address this concern.

Sulphur Bank Mine

The U.S. EPA declared the Sulphur Bank Mine in the East Lake Communities Planning Area a Superfund site in 1991. Since 1980, the EPA's Superfund program has managed the cleanup of hazardous waste sites across the United States with the goal of protecting human and environmental health. The process typically involves identifying the responsible party (establishing liability), suing responsible parties to pay for cleanup work, involving affected communities and facilitating their participation, and returning Superfund sites to productive use.

Mining activities at the Sulphur Bank Mine site resulted in the distribution of waste across the site and into Clear Lake, which contaminated soils, surface water, and groundwater, posing risks to human and environmental health; this activity also deposited mercury into Clear Lake's sediment, which has collected in fish and sometimes made them unsafe to eat. Contaminants found in the mine tailings and waste rock at the site included mercury, arsenic, and antimony. Mercury, a neurotoxin, affects the nervous system, while arsenic can cause cancer and affect organ function. Antimony is poisonous and cancer-causing at high concentrations. In the 1970s, the Bureau of Indian Affairs used contaminated mine wastes as residential and infrastructure construction material at the adjacent Sulphur Bank Rancheria, which potentially compounded negative disparities in health outcomes for Elem Pomo of the Sulphur Bank Rancheria. Surface water and groundwater discharge from the mine property have also contaminated Clear Lake and the Planning Area's wetlands north of the site.

To focus its cleanup efforts, the EPA divided the Sulphur Bank Mine site into three Operable Units (OUs): OU 1 includes the mine site and residential soils to the north and south, OU 2 encompasses Clear Lake and its sediment, and OU 4 consists of the wetlands north of the mine site and the surrounding land. What was once OU 3 was divided amongst the other OUs. Since the EPA’s oversight began in 1991, the agency has initiated various “Response Actions,” including implementing flood-control measures, constructing surface water diversions, resealing geothermal wells, and excavating mine wastes from residential areas and roadways.

In 2023, the EPA issued its final plan to clean up OU 1. It focuses on protecting nearby residential areas, ensuring residents’ long-term health and safety, and making on-site mine areas safe for limited use by Elem Indian Colony residents for activities like hunting, fishing, foraging, and movement to and from nearby lands. The EPA has not published proposed cleanup plans for OU 2 and OU 4. The agency continues to monitor Clear Lake and its sediment (OU 2) to determine how best to address contamination, assessing which options will address mercury levels in fish and determining how mercury from mine waste differs from naturally occurring mercury in the lake. At the North Wetlands (OU 4), the EPA, in collaboration with the Elem Indian Colony, initiated soil, sediment, groundwater, and organism testing for mercury and arsenic in 2022 and 2023. Results will be incorporated in a “Remedial Investigation” that will inform viable cleanup options for the area.

Policy guidance related to hazardous waste and materials is included in the Health and Safety Element.

Access to Public Facilities and Services

Due to the East Lake Communities Planning Area’s remote location and rural setting, access to vital public services is sometimes limited, which can be exacerbated for Equity Priority Areas and Populations that face economic constraints. Specific issues facing the Planning Area’s Equity Priority Areas and Populations include long response times for emergency services, like police, fire, and ambulance services; access to reliable power (an acute issue for those with power-dependent medical needs); and affordable garbage collection and disposal services. Some households in the Planning Area lack adequate water and sewer service and infrastructure.

Policies and actions addressing standards for emergency response times and power shutoffs are found in the Health and Safety Element. Policies and actions addressing water, sewer, and solid waste disposal services are found in the Public Facilities and Services Element.

Food Security

Equity Priority Areas and Populations both face barriers in accessing healthy food, including fresh produce and minimally processed foods. The location and distribution of retail outlets selling these foods, transit accessibility to existing locations, and cost are all key determinants in whether the food is truly accessible. In

Healthy food in this General Plan refers to foods containing fruits, vegetables, whole grains, lean proteins, and/or healthy fats and limited amounts of saturated fat, sodium, or added sugar.

addition to increasing access to healthy food, community members are also interested in educational and economic opportunities related to local agriculture that could empower communities to grow and/or sell their own food.

In addition to the goals, policies, and actions in this element addressing food access, the Land Use Element includes strategies to develop new grocery stores and other fresh food outlets, and the Circulation Element includes policy guidance to optimize transit options to key destinations.

Healthy Living Spaces

Housing affordability is an issue throughout the state. In Lake County, overcrowding is prevalent due to high housing costs and has the potential to pose mental and physical health risks for Equity Priority Areas and Populations, including spreading communicable disease.

Older homes often have a greater risk of having lead-based paint, asbestos, and/or mold, which can all pose risks to human health. Local public health advocates have also identified exposure to toxins, such as aerosols and tobacco smoke, in multifamily housing as an environmental justice issue throughout the county, in addition to tobacco use generally as a health hazard. The issue of toxins in multifamily housing is especially acute for children and other sensitive residents living in multifamily residential buildings where “third-hand” smoke can seep through walls, vents, and plumbing. Low-income residents with less access to healthcare and other resources are particularly vulnerable to health risks from smoking and from exposure to second- and third-hand smoke and aerosols.

Abandoned Properties and Illegal Dumping Issues

Along the shoreline of Clear Lake, there are informal and dilapidated structures that not only impact the area’s scenic and aesthetic value but also pose public health concerns. Illegal dumping is also an issue in the East Lake Communities Planning Area, warranting policies that provide incentives for junk removal and structure improvements. Policy guidance related to scenic impacts is provided in the Open Space, Conservation, and Recreation Element, while the Land Use Element and Public Facilities and Services Element include policy guidance related to code enforcement and illegal dumping.

Access to Healthcare

Healthcare access is limited and an increasing need in the East Lake Communities Planning Area as the population ages. Lack of access to medical professionals, clinics, hospitals, and in-home care, along with having to drive long distances for medical services, poses difficulties for residents. This is of particular concern for Equity Priority Populations, especially older residents living on fixed incomes and/or with disabilities who may have limited mobility options but need more frequent access to medical services.

Access to Recreation

Residents in the East Lake Communities Planning Area need more active recreation opportunities, especially for youth. While community members support expanded recreation opportunities for all residents, public recreation facilities and after-school programs are particularly important for Equity Priority Areas, whose residents often lack resources to pay for private extracurricular activities and may have limited mobility options to access recreation opportunities more than a short distance away.

See the Open Space, Conservation, and Recreation Element for information and policy guidance discussing development of multipurpose trails, park system expansion, community participation in park design, and broadening of recreation opportunities in the Planning Area.

Opportunities for Community Engagement

Low-income residents and other Equity Priority Populations face difficulties engaging in civic processes, such as the General Plan Update or an individual development proposal, due to barriers like transportation, technological literacy, and internet access, and having time available to be involved. Community members who do not speak English, or those with limited English proficiency, are often precluded from engaging in civic processes when translation services are not available.

In addition to the community engagement policies addressed in this element, the Public Facilities and Services Element also includes policy guidance to create a more informed citizenry by improving telecommunications services.

Environmental Justice Issues in Tribal Communities

Environmental justice for tribal communities requires recognition of the historical and ongoing impacts of colonization, displacement, and land-use inequities that continue to affect tribal communities. While Tribal communities continue to live in, steward, and preserve sacred cultural resources and practices in the region, they also continue to face disproportionate barriers and impacts with respect to the Community-Identified Issues discussed in the above subsections, and additionally with respect to Tribal sovereignty and access to Tribal Cultural Resources and ancestral land. For example, in addition to posing risks to human health, environmental degradation and pollution also pose risks to Tribal Cultural Resources such as sacred sites, traditional plant gathering areas, and waterways used for cultural practices and livelihoods. Similarly, investments, infrastructure improvements, development of parks and recreation facilities, and other measures to address the Environmental Justice issues described above must be planned and implemented in collaboration with Tribal governments and the broader community in a way that upholds Tribal sovereignty and avoids disturbances to culturally significant landscapes. Environmental justice for tribal communities includes the protection of Tribal Cultural Resources and ancestral lands as a fundamental component of community health, identity, and environmental balance, in addition to supporting tribal community health and access to clean air and water, healthcare, economic opportunity, and safe homes. This involves prioritizing environmental restoration and contamination cleanup in areas of cultural significance or ancestral use, ensuring Tribal participation throughout the planning and policy process particularly in aspects that affect culturally important lands and resources,

and promoting tribal-led environmental stewardship programs and access to funding for sustainable land management and habitat restoration practices. Ongoing collaboration with Tribal governments supports a just and inclusive approach to planning that recognizes cultural survival, environmental stewardship, and tribal self-determination as fundamental components of community well-being. Related goals, policies, and actions addressing these concerns are included in the Agricultural Resources, Water Resources, Health and Safety, and Open Space, Conservation, and Recreation Elements.

Goals, Policies, and Actions

Goal EL-EJ-1

Communities that promote high quality of life and good health outcomes for everyone, including Equity Priority Populations.

Policies

EL-EJ-1.1

Promote efforts to improve community health outcomes and ensure that County investments support community health goals whenever possible.

Refer to the Health and Safety Element for additional policies pertaining to community health.

EL-EJ-1.2

Continue to work with the EPA to support remediation of the Sulphur Bank Mine Superfund site to protect community health and restore a safe and healthy environment.

EL-EJ-1.3

Limit exposure to secondhand smoke, including from electronic smoking devices, tobacco smoke, and cannabis smoke, and enforce State and local laws and regulations pertaining to prohibitions of tobacco and cannabis use in specified locations and selling tobacco and cannabis to minors.

EL-EJ-1.4

Lead and support activities to facilitate affordable access to health care, including addressing infrastructure and service barriers to broadband access for telehealth and multimodal access to clinics.

EL-EJ-1.5

Encourage and support the expansion of medical and senior care facilities, such as medical clinics, urgent care, emergency care, behavioral health facilities, and pharmacies, prioritizing areas that lack access to healthcare. Strategies could include incentives or streamlining efforts.

EL-EJ-1.6

Support facilities enhancements to optimize libraries and other public spaces, such as senior centers, to support telemedicine appointments.

EL-EJ-1.7

Support efforts to attract medical professionals to the East Lake Communities Planning Area.

EL-EJ-1.8

Collaborate with neighboring jurisdictions to assess and address issues related to capacity and access to emergency, acute, and preventative/primary healthcare services.

EL-EJ-1.9

Encourage and support efforts to expand access to community health workers, ~~promotes~~promoters, peer navigators and support specialists, and neighborhood-based mutual aid networks that reduce social isolation; strengthen community connections; and connect residents to services, benefits, food resources, telehealth, and civic processes before challenges escalate into crises.

EL-EJ-1.10

Consider addressing behavioral health, trauma recovery, grief support, and substance use prevention when designing post-disaster recovery social programs.

~~EL-EJ-1.9~~EL-EJ-1.11

Design and improve County facilities and programs to meet the needs of older adults and other Equity Priority Populations.

~~EL-EJ-1.10~~EL-EJ-1.12

Work with local Tribal governments and local community organizations on mitigation strategies for asthma-related environmental hazards.

Actions

EL-EJ-1.a

Establish a process for assisting community-based organizations and public-private partners with obtaining financing (e.g., grants) to implement physical improvements and beautification projects such as public art, fire-resilient vegetative buffers, and planting strips in Equity Priority Areas.

EL-EJ-1.b

Consider adopting a tobacco and cannabis retail licensing fee that earmarks a portion of the fee for enforcement of tobacco- and smoking-related policies.

EL-EJ-1.c

Consider adopting an ordinance prohibiting smoking (including vaping, electronic cigarettes, tobacco, and cannabis) in specific areas, such as:

- Inside and near doors and windows of multifamily housing units to limit residents' exposure to the harmful effects of second-hand smoke.
- At worksites, including in and near unenclosed spaces of employment, outdoor dining areas, and unenclosed waiting areas.

-
- On public property, including in public parks and trails, and at public events, such as farmers' markets and community fairs.

EL-EJ-1.d

Consider adopting an ordinance aimed at reducing instances of youth using tobacco products that might include prohibiting the location of tobacco retailers near schools and other youth-serving facilities and/or establishing restrictions on the placement and promotion of tobacco products so they are not easily accessible to youth.

EL-EJ-1.e

Establish a Commission on Aging and Adult Services to review and make recommendations to the Board of Supervisors on specific issues, policy questions, and funding resources relating to older adults, including senior centers and senior service provider organizations, among others.

EL-EJ-1.f

Work with the newly formed Commission on Aging to prepare a new Chapter of the County's Code of Ordinances for "Community Services" to detail services available in the county, including support services for older adults.

EL-EJ-1.g

Explore adoption of an elder income index and/or other mechanisms to provide cost relief for the older adult residents in the county.

EL-EJ-1.h

Work with North Coast Opportunities (NCO) to activate the AmeriCorps/Senior Corps Retired Senior Volunteer Program (RSVP) to collect volunteer hours and use those as in-kind monetized matching funds, as allowed by the State and federal RSVP programs, to maximize funding for needed community services.

Goal EL-EJ-2

Access to living spaces that are safe, clean, and healthy for all East Lake Communities Planning Area residents.

Policies

EL-EJ-2.1

Ensure housing in the East Lake Communities Planning Area provides safe and sanitary environments for residents.

EL-EJ-2.2

Ensure that future improvements in Equity Priority Areas will not result in a net loss of subsidized affordable housing or significant preventable displacement of residents.

EL-EJ-2.3

Ensure adequate living conditions for tenants through ongoing and effective code enforcement, with priority given to housing in Equity Priority Areas.

EL-EJ-2.4

Work with and support agencies and organizations providing supportive services to individuals experiencing homelessness, including efforts to secure temporary and long-term housing for unhoused community members.

Actions

EL-EJ-2.a

Pursue all available federal and State funds ~~and create an assistance program to assist with support~~ housing preservation and rehabilitation, with a focus on ~~identifying funding to remediate~~ health and safety issues, ~~addressing code violations, and improving livability~~ for housing in Equity Priority Areas and housing units occupied by Equity Priority Populations. ~~Create an assistance program for homeowners and landlords to bring older properties up to code and improve their livability, with priority given to homes in Equity Priority Areas and those housing Equity Priority Populations.~~

EL-EJ-2.b

Review and update the Zoning Ordinance to remove unnecessary barriers to the development of farmworker housing.

EL-EJ-2.c

~~EL-EJ-2.d~~ Amend the Zoning Ordinance to adopt development and maintenance standards for farmworker housing.

~~Create an assistance program for homeowners and landlords to bring older properties up to code and improve their livability, with priority given to homes in Equity Priority Areas and those housing Equity Priority Populations.~~

EL-EJ-2.eEL-EJ-2.d

Develop a system or other mechanism to inventory rental housing in the county by address, both to track the stock of rental housing and to develop a mailing list to reach rental housing occupants. Additional details in the inventory may also include property owner contact information.

EL-EJ-2.fEL-EJ-2.e

Conduct periodic outreach in Equity Priority Areas and to Equity Priority Populations to inform tenants of their rights and responsibilities. Written outreach efforts should be provided in English and Spanish.

EL-EJ-2.gEL-EJ-2.f

Work to identify appropriate camping options for unhoused individuals and eliminate camping where prohibited.

Goal EL-EJ-3

Convenient and affordable access to healthy food for all East Lake Communities Planning Area residents.

Policies

EL-EJ-3.1

Support efforts to establish and/or expand programs and other activities to improve food security and eliminate hunger, including food assistance programs, community gardens, [Tribal food gardens](#), and school lunch access.

EL-EJ-3.2

Support transit route changes or expansions and active transportation network improvements that link Equity Priority Areas to healthy food sources, including grocery stores. In addition, encourage new healthy food retailers near existing transit connections.

EL-EJ-3.3

Expand local food access and agriculture by supporting and encouraging farmers' markets, urban farming, community gardening, and other similar activities.

EL-EJ-3.4

Encourage new development projects to incorporate community gardens into the project design, as appropriate.

EL-EJ-3.5

[Encourage and support cooking classes educating the public on how to use fresh and/or locally produced ingredients.](#)

Actions

EL-EJ-3.a

Develop incentives to encourage the establishment of grocery stores and other venues selling healthy foods, preferably locally grown or produced, in areas with limited access to healthy food, prioritizing locations in or near Equity Priority Areas.

EL-EJ-3.b

Work with certified farmers' markets to support acceptance of payment via EBT (Electronic Benefits Transfer) to maximize access, prioritizing markets in or near Equity Priority Areas.

EL-EJ-3.c

Inventory and map food deserts in the East Lake Communities Planning Area to inform future policy and decision making to address food access barriers.

EL-EJ-3.d

Streamline permitting processes for grocery stores and markets in Equity Priority Areas and food deserts.

Goal EL-EJ-4

Effective decision making that seeks and incorporates the voices of East Lake Communities Planning Area residents, including from Equity Priority Populations and those living in Equity Priority Areas.

Policies

EL-EJ-4.1

Empower all residents to participate in local decision making and ensure meaningful public engagement processes and events that make it possible for everyone—especially Equity Priority Populations and those living in Equity Priority Areas—to participate and influence outcomes.

EL-EJ-4.2

Involve the Municipal Advisory Council in plan development, including planning efforts that involve public outreach.

EL-EJ-4.2EL-EJ-4.3

Use multiple channels—including digital and print media, County websites, local radio, and cable TV—to share publicly important information and publicize opportunities for community engagement to ensure access for everyone, including those without home computers or internet.

EL-EJ-4.3EL-EJ-4.4

Provide interpretation and translation services for community announcements, community meetings, services, and programs, to ensure accessibility for all community members, as appropriate.

EL-EJ-4.4EL-EJ-4.5

Encourage sponsors of major development projects to have early and frequent communication, in the appropriate language(s), with affected residents and interested parties.

EL-EJ-4.5EL-EJ-4.6

Strive for more diverse representation on boards, commissions, committees, and similar bodies to better reflect the demographics and varying viewpoints of East Lake Communities Planning Area residents, including representatives of Equity Priority Populations and/or other underrepresented groups, as appropriate.

EL-EJ-4.6EL-EJ-4.7

Engage young people in long-range planning processes and other County-administered programs and processes through outreach and partnerships with libraries, schools, and local youth organizations when possible.

Actions

EL-EJ-4.a

Hold periodic public meetings in each Equity Priority Area to hear from residents of these communities about issues impacting their quality-of-life outcomes and use that input to inform the County's annual budgeting process.

EL-EJ-4.b

Support Municipal Advisory Councils [and their subcommittees](#) to develop best practice guidelines to support all County departments with conducting effective outreach in the East Lake Communities Planning Area, such as how to identify appropriate engagement activities and practices based on the project or issue type and stakeholders involved, a list of stakeholders and/or community leaders in the area, and language interpretation or translation services to offer.

EL-EJ-4.c

Explore options and seek funding to transition to accessible outreach materials for those with sensory disabilities (e.g., visually impaired and/or deaf or hard of hearing).

EL-EJ-4.d

To inform existing and potential future Municipal Advisory Councils (MACs), adopt a policy detailing the process for forming a MAC and the roles, responsibilities, and authority of a MAC, which may take inspiration from Mendocino County Policy 51, Formation/Operation of Municipal Advisory Councils.

EL-EJ-4.e

Periodically update Equity Priority Area maps, including the Tribal Lands boundaries as updated by the Bureau of Indian Affairs, to ensure that the appropriate communities are prioritized as part of the County's work to implement the Environmental Justice Element.

Public Facilities and Services

Public facilities are buildings, pipes, pumps, roadways, and other infrastructure that support community living. Public services include fire protection, law enforcement, waste collection, education, and other vital services the County and other public agencies provide to residents. Access to public facilities and services is a major ongoing priority and key issue in the East Lake Communities Planning Area. Aging infrastructure, especially aging water infrastructure, is of particular concern. This element prioritizes the provision of a high and healthy quality of life for the Planning Area's residents by enhancing public services, improving infrastructure, and improving access to and affordability of these utilities and services.

The Public Facilities and Services Element addresses the publicly provided facilities and services in the Planning Area and presents information and policy guidance to maintain and, when possible and appropriate, enhance the Planning Area's facilities and services. The following topics are covered:

- Fire Protection and Police Services
- Water and Wastewater Service
- Stormwater Management
- Solid Waste Disposal
- Communications Systems
- Educational Facilities and Services
- Disadvantaged Unincorporated Communities

These topics are an integral part of the County's planning strategy and an important consideration when considering growth and development in the East Lake Communities Planning Area.

Many services are provided by agencies and organizations not under the purview of the County, so Lake County has limited direct control over their services, though this element includes Goals, Policies, and Actions to guide the County's collaboration with external entities and other efforts aimed at making sustainable, high-quality services available to the community.

This element also considers public facility and service needs and deficiencies in low-income areas considered to be disadvantaged unincorporated communities based on State guidance.



Woodland Community College Lake County Campus in Clear Lake and Mendocino College Lake Center in Lakeport provide access to higher learning to Lake County residents. The Clearlake Oaks County Water District owns this water tower with a mural of Clear Lake and Mount Konociti, which can be seen from Highway 20. -

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PF&S 6

Fire Protection and Police Services

Fire districts in the county respond to structure and wildland fires, vehicular accidents, technical rescue, hazardous material incidents, and medical emergencies. Fire protection and emergency medical services for the East Lake Communities Planning Area are provided by the Northshore Fire Protection District (FPD) and the Lake County FPD.

The Northshore FPD services the majority of the Planning Area as well as the adjacent North Lake Communities Planning Area. It covers a service area of approximately 357 square miles and is the largest fire district in the county. Northshore FPD operates one station within the East Lake Communities Planning Area: Station 75, located at 12655 East Highway 20 in Clearlake Oaks. The Northshore FPD operates three other stations in the North Lake Communities Planning Area, including one in the community of Lucerne [\(unmanned in 2026\)](#) immediately north of Kono Tayee- [and one in Spring Valley](#). It employs 22 staff members: 19 full-time emergency medical/firefighting employees and three full-time administrative employees.

The Lake County FPD's jurisdiction is approximately 165 square miles and includes the City of Clearlake, some of its surrounding areas, and portions of the Lower Lake Planning Area to the south. The Lake County FPD manages two stations, both of which are in the Lower Lake Planning Area. Lake County FPD employs 26 staff members. [The California Department of Forestry and Fire Protection \(CAL FIRE\), the third largest fire protection district in the state, also operates a station in the community of Clearlake Oaks.](#)

Both Northshore FPD and Lake County FPD also have volunteer staff members and mutual-aid agreements between the districts, [California Department of Forestry and Fire Protection \(CAL FIRE\)](#), and surrounding fire districts.

The Lake County Sheriff's Office provides law enforcement, patrol, and coroner services to the East Lake Communities Planning Area. The Planning Area's nearest law enforcement facility is in the City of Clearlake. The California Highway Patrol (CHP) also serves the East Lake Communities Planning Area, providing traffic control, accident investigation, and vehicle licensing for State highways. CHP collaborates with the Sheriff's Office when requested during joint operations and emergencies.

See also the Health and Safety Element for more details and policy guidance related to wildfire hazards and emergency response.

Water and Wastewater Service

[Potable water within the Planning Area is supplied from various sources, including private wells, municipal or district wells, springs, and surface water from Clear Lake.](#) There are eight public water systems in the Planning Area that are regulated by the State Water Resources Control Board, Division of Drinking Water. These systems fall within one of two designation types:

- **Community water systems** have at least 15 service connections used by yearlong residents or regularly serve at least 25 or more yearlong residents, including subdivisions, mutual water companies, manufactured home parks, and similar residential areas.

- **Transient, non-community (TNC) water systems** regularly serve less than 25 of the same people for six or more months of the year, including restaurants, motels, campgrounds, small grocery and other retail stores, and other nonresidential uses.

The water systems in the East Lake Communities Planning Area are:

- **Clearlake Oaks County Water District**, a community water system that provides potable water to single-family homes, lodging, restaurants, and retail destinations in the Clearlake Oaks ~~community~~ and Paradise Cove communities. The system sources surface water from an intake at Clear Lake.
- **County Service Area (CSA) #2 – Spring Valley**, a community water system that serves the Spring Valley Lakes subdivision, obtaining its water from an infiltration intake at the confluence of Cache and Wolf Creek. The water system has nearly 500 residential connections.
- **County Service Area #13 – Kono Tayee**, a community water system that provides potable water to residential and commercial connections at Kono Tayee Point along the eastern shore of Clear Lake. The system sources water from two groundwater wells.
- **Glenhaven Mutual Water Company**, a community water system that sources water from two groundwater wells between Clear Lake and SR 20 and provides water to the community of Glenhaven, with approximately 120 residential connections.
- **Windflower Point Mutual Water Company**, a community water system that services Windflower Point, a community in the “Gooseneck” area west of the City of Clearlake. The system, which sources water from a groundwater well, has 25 residential connections.
- **Brassfield Estate Winery**, a TNC system with residential and commercial connections. Water is sourced from groundwater by a well.
- **Cache Creek Vineyards**, a TNC system that relies on a groundwater well and provides water to a tasting room and winery.
- **Indian Beach Resort**, a TNC system along the shore of Clear Lake that uses groundwater and provides potable water to residences.

The Lake County Environmental Health Department (EH) issues permits for all groundwater wells in the county and regulates State Small Water Systems, which have between 5 and 14 connections. EH also inspects and permits water systems that serve food establishments with private water systems, such as bed and breakfast facilities.

Wastewater service consists of transmission of wastewater to a treatment facility, treatment, and then disposal of the wastewater and residual waste solids. Three public sewer systems service the Planning Area’s major communities: the Northwest Regional Wastewater System, Southeast Regional Wastewater System, and Clearlake Oaks County Water District.

The remainder of the East Lake Communities Planning Area, including the community of Glenhaven and the Spring Valley Lakes subdivision, relies on on-site wastewater treatment systems (OWTS), commonly known as septic or leach field systems. These systems require permits from EH, who conduct site

evaluations to determine whether each site is appropriate for the installation of a new OWTS and the type of system needed.

Many of the existing water systems obtain water directly from the lake, which must then be treated to make it potable. Some of these water systems, as well as some parts of the sewer systems, can become inundated when lake levels become abnormally high, leading to closure of these facilities. The Health and Safety Element discusses flooding and inundation and includes goals and policies aimed to protect and support people and critical facilities during heavy rainfall events.

Stormwater Management

The East Lake Communities Planning Area, like the rest of the county, has areas prone to flood risk. These risks are described and addressed in the countywide Health and Safety Element; however, this element addresses the related issue of stormwater management, or the public policies, activities, and infrastructure employed to regulate the rate, volume, and quality of runoff. Lake County uses a variety of facilities and methods to manage stormwater, including:

- **Levees**, or high ridges along waterways and reservoirs to provide flood control.
- **Stormwater detention facilities**, which are designed for the temporary storage of stormwater, working to attenuate peak runoff and provide water quality treatment benefits by allowing particulates to settle in the basin.
- **Groundwater recharge facilities**, which move water from the ground's surface into aquifers, which can occur naturally or through human-controlled methods.
- **Stormwater retention**, the process of collecting and storing rainwater, snowmelt, and other storm runoff to prevent flooding.

Traditional development that involves removing vegetation and soil and installing impermeable surfaces (like asphalt and concrete) increases stormwater runoff and can exacerbate flood risk, so the General Plan encourages low-impact development and green infrastructure.

Low-impact development refers to systems and practices that use or mimic natural processes that result in the infiltration, evapotranspiration, or use of stormwater to protect water quality and associated aquatic habitat. **Green infrastructure** refers to the management of wet-weather flows that use these processes, and the patchwork of natural areas that provide habitat, flood protection, cleaner air, and cleaner water. At both the site and regional scale, low-impact development/green infrastructure practices aim to preserve, restore, and create green space using soils, vegetation, and rainwater harvest techniques. Examples of green infrastructure include rain harvesting systems, bioswales (channels that provide stormwater treatment and retention), permeable pavements, green roofs, and urban tree canopies.

- United States Environmental Protection Agency, 2024

Solid Waste Disposal

Most of the East Lake Communities Planning Area is serviced by Lake County Waste Solutions. Lake County Waste Solutions transports garbage, recycling, and green waste to the Lake County Transfer Station in Lakeport. Solid waste is then transported to the Eastlake Sanitary Landfill east of the City of Clearlake. Tire waste is transported off-site to Ukiah. Quackenbush Mountain Resource Recovery and Compost facility processes compostable material and construction debris collected in the Planning Area.

Illegal dumping and open, outdoor storage of trash remains a concern on remote property, private property, and roadways around the East Lake Communities Planning Area. The County has launched community clean-up programs, free dumping days, and public outreach campaigns to combat this issue.

The County's efforts are guided by the Countywide Integrated Waste Management Plan (CIWMP), mandated by State law under AB 939. The CIWMP outlines local waste diversion and disposal conditions while establishing programs to meet state waste diversion goals. The CIWMP serves as the County's primary tool for developing countywide waste reduction programs and addressing landfill needs.

In alignment with the CIWMP, the County's Solid Waste Management Plan outlines additional strategies to increase diversion rates, expand green waste composting programs, and comply with State recycling mandates. Despite these initiatives, unincorporated areas of Lake County, including the East Lake Communities Planning Area, face challenges in meeting waste diversion goals, as reflected in CalRecycle data. However, ongoing education, enforcement, and expanded services aim to address these gaps and foster more sustainable waste practices in the region.

Communications Systems

Telephone services in the East Lake Communities Planning Area are available from numerous providers. Cellular service is provided by major carriers like Verizon, AT&T, and T-Mobile. While there are telecommunications towers in the Planning Area, cell phone coverage is limited and mostly near the populated areas along SR 20; the Spring Valley community faces significant cell coverage limitations. Landline services are available throughout the Planning Area.

According to the Federal Communications Commission (FCC) National Broadband Map, high-speed internet access (i.e., with download/upload speeds at or above 25/3 megabytes-per-second) are mostly limited to the populated areas immediately adjacent to the length of SR 20 beginning from the northern boundary with the North Lake Communities Planning Area until the intersection with SR 53 east of Clearlake. Service becomes very limited for households further and up the hill from SR 20. The eastern parts of the Planning Area mostly lack high-speed internet service except for in some larger communities, like Spring Valley. Many East Lake Communities Planning Area residents are concerned about limited cell phone and high-speed internet service, and many rely on Starlink as their primary internet provider.

Educational Facilities and Services

The East Lake Communities Planning Area is served by three school districts. The majority of the Planning Area is served by the Konocti Unified School District. The Lucerne Elementary School District covers a small northwestern part of the Planning Area, but all schools in the District are in the neighboring North Lake Communities Planning Area. The northern portion of the Planning Area is in the Upper Lake Unified School District, which also lacks any schools within the Planning Area.

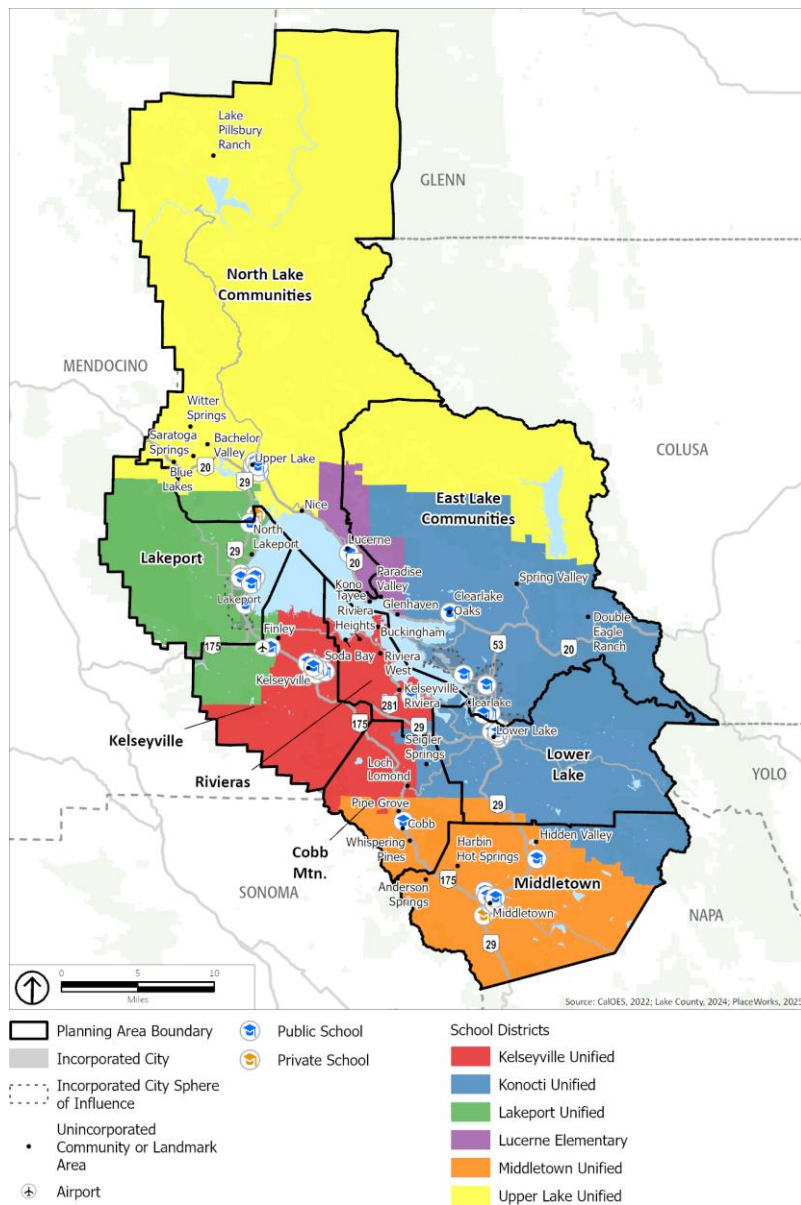
The majority of the Konocti Unified School District is encompassed within the Planning Area. The District operates four elementary schools and one middle school in the Planning Area: Burns Valley School, East Lake School, Lower Lake Elementary, Pomo School, and Obsidian Middle School. East Lake School and Lower Lake School are in the Planning Area and the others are within the Clear Lake city limit. Konocti Unified School District also operates Lower Lake High School, which is situated in the Lower Lake Planning Area and serves East Lake residents, and several alternative education schools serving grades K-12 in the City of Clearlake, but there are no high schools in the East Lake Communities Planning Area. School locations are depicted in **Figures EL-15** (countywide) and **EL-16** (East Lake Communities Planning Area). Woodland Community College – Lake County Campus is in Clearlake and is part of the greater Yuba Community College District.

The Redbud Library is in the City of Clearlake. This 13,000-square-foot facility was built in 1995 and offers free public Wi-Fi, 11 public computers, and a public access printer and copy machine. The library also provides numerous critical public services, like bus and park passes, work training programs, and after school programs. The library also has a large community meeting room, which is available to rent for public meeting use. Live Oak Senior Center, in Clearlake Oaks, hosts various events and provides non-profit services to older adults in the area, including fresh meals Monday through Friday. The Senior Center features other food and activity programs for older adults as well as several community meeting rooms.

Disadvantaged Unincorporated Communities

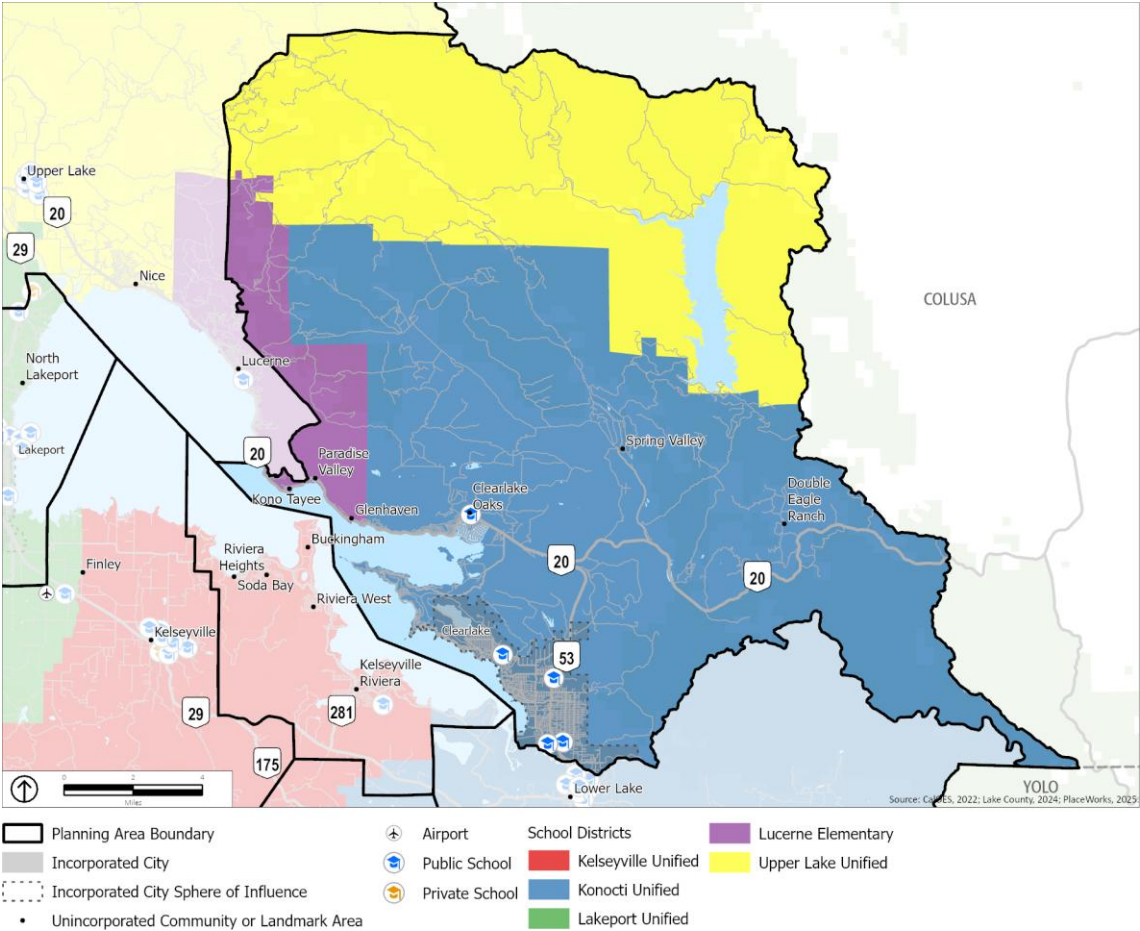
Through SB 244, State law requires that General Plans address the needs of disadvantaged unincorporated communities (DUCs), which are unincorporated communities with an annual median household income that is less than 80 percent of the statewide annual median household income and either within an incorporated jurisdiction's sphere of influence (SOI), an island within an incorporated jurisdiction's boundary, or geographically isolated and has existed for at least 50 years—these are known as legacy communities. DUCs often lack adequate infrastructure to sustain public health and safety, which can foster economic, social, and educational inequality. Therefore, the law requires that counties and cities identify DUCs, analyze infrastructure and fire service needs and deficiencies, and assess potential funding mechanisms for needed expansions of services and facilities. Counties are responsible for conducting these analyses and planning for the needs of legacy community DUCs, while cities are responsible for addressing these requirements for DUCs in their SOIs. With each update to the County's Housing Element, the County is required to identify DUCs, evaluate their infrastructure and service deficiencies, and assess potential funding mechanisms to address those deficiencies. The DUC analyses are summarized in **Appendix B, Disadvantaged Unincorporated Communities**. As required by State law, this Public Facilities and Services Element includes goals, policies, and actions to address infrastructure deficiencies, as they are identified in each update to the DUC analysis.

Figure EL-15 Countywide Schools



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Figure EL-16 East Lake Community Planning Area Schools



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Goals, Policies, and Actions

Goal EL-PF-1	Resilient and well-maintained public facilities and services that meet the needs of all community members, are adequately funded, and enhance quality of life in the East Lake Communities Planning Area.
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Policies

EL-PF-1.1

Ensure that all neighborhoods have access to safe, well-maintained public facilities, utilities, and services that meet community needs now and as the community grows.

EL-PF-1.2

Prioritize County expenditures on infrastructure, public facilities, services, recreation and open space amenities, [resilience centers](#), and environmental enhancements that benefit and respond to the needs of Equity Priority Populations and residents of Equity Priority Areas.

EL-PF-1.3

Work with local special districts, ~~and~~ local Tribal governments [and local community organizations](#) to coordinate public service improvements in the East Lake Communities Planning Area.

EL-PF-1.4

Continue to assess existing and future needs for public facilities and services, such as internet access, meeting rooms, and libraries, and work to develop, retrofit, or expand facilities and services, as necessary, to meet those needs, prioritizing the needs of Equity Priority Populations and residents of Equity Priority Areas.

EL-PF-1.5

[Support expansion of mobile library van services into the East Lake communities.](#)

~~EL-PF-1.5~~EL-PF-1.6

Work with utility service providers [and property owners](#) to ensure service and program offerings align with existing and planned development patterns to meet current and future community needs.

~~EL-PF-1.6~~EL-PF-1.7

Coordinate with public utility providers to acquire adequate rights-of-way to facilitate expansion of services in a timely manner.

~~EL-PF-1.7~~EL-PF-1.8

Require new subdivisions in the Community Growth Boundary to include dark sky compliant streetlights.

EL-PF-1.8EL-PF-1.9 *

Require new development projects to provide their own infrastructure, tie into existing infrastructure, or be required to provide infrastructure that will tie into future infrastructure, as determined by the County and other affected agencies.

EL-PF-1.9EL-PF-1.10 *

Prior to project approval, ensure that new development projects will not create significant adverse impacts on existing infrastructure by implementing County-approved mitigation measures to reduce the impact.

EL-PF-1.10EL-PF-1.11

Ensure new development projects pay their fair share of public improvement costs for infrastructure, facilities, and services.

EL-PF-1.11EL-PF-1.12

Require a cost/benefit analysis for major developments to determine the fiscal impact of projects on public services and other community infrastructure.

EL-PF-1.12EL-PF-1.13

Continue to support the Department of Animal Care and Control in its efforts to minimize pet overpopulation and abuse.

Actions

EL-PF-1.a

Upon each update to the Housing Element, perform an analysis of infrastructure needs and deficiencies in disadvantaged unincorporated communities (DUCs), explore funding mechanisms that could make extension of needed services and facilities feasible, and update the associated information, policies, and actions of the General Plan as needed to reflect new conditions.

EL-PF-1.b

In cooperation with other service providers as appropriate, develop long-range facility master plans for relevant public services.

EL-PF-1.c

Consider the creation of additional assessment districts, county service areas, and/or special districts to fund and manage new and improved infrastructure, where needed.

EL-PF-1.d

Update and maintain policies and procedures that govern the County's use of artificial intelligence (AI) to ensure the County's and community's data is protected, appropriate oversight of any AI-generated products or activities, and that the use of AI does not conflict with the best interest of the County or its constituents.

Goal EL-PF-2

Public facilities that are designed to provide quality service while minimizing impacts on the environment.

Policies

EL-PF-2.1

Require public facilities installations and extensions to be sited and designed to minimize or avoid potential adverse impacts to the community and environment.

EL-PF-2.2

When infrastructure, such as sewer, water, and transmission lines, must cross agricultural lands, locate the infrastructure in the public right-of-way or along lot lines whenever possible.

EL-PF-2.3

Coordinate with utility companies to minimize the visual and cultural impact of new or expanded facilities through appropriate facility sizing, design, and placement on the landscape. Generally encourage utility lines to be co-located and to follow property lines and avoid residential areas, ridgetops, open space, and other sensitive areas.

EL-PF-2.4

Require mitigation for inter-county regional transportation facilities, high-voltage electrical transmission lines, and energy or water pipelines that do not serve the local population and provide little or no long-term benefit to the local affected area.

EL-PF-2.5

Minimize vulnerability of public service facilities to natural and human-caused hazards and threats.

EL-PF-2.6

Encourage public and other facilities designed for direct public use, such as libraries, to be located along major thoroughfares and/or near the center of general community activities.

EL-PF-2.7

Design new or redeveloped governmental and civic facilities to accommodate multiple community uses, such as recreation and community gathering spaces, and to be accessible to all users, regardless of age, ability, race, ethnicity, culture, language, gender identity, or economic status.

EL-PF-2.8

Encourage the development of joint school facilities, recreational facilities, and educational and service programs between school districts and other public agencies.

EL-PF-2.9

Encourage joint use of existing right-of-way corridors and utility easements.

EL-PF-2.10

Ensure that County-managed spaces and facilities support a feeling of safety for users and require new development projects to provide adequate lighting, clearly marked crosswalks, and other safety-promoting features in public spaces, as appropriate.

Goal EL-PF-3

Fire protection and law enforcement services that protect community members and property.

Policies

EL-PF-3.1 *

Work with the Sheriff's Office, Northshore Fire Protection District, Lake County Fire Protection District, and CAL FIRE to support each agency's ability to maintain adequate staffing and incident response times in accordance with State and federal guidelines.

Emergency preparedness and response is further addressed in the Health and Safety Element.

EL-PF-3.2

Locate new fire and police stations to minimize response times to the East Lake Communities Planning Area, while prioritizing effective use of existing facilities.

EL-PF-3.3

Support efforts to expand fire protection service to continue to meet the needs of the East Lake Communities Planning Area.

EL-PF-3.4

Continue to promote cooperative fire protection agreements with CAL FIRE to provide adequate fire protection throughout the East Lake Communities Planning Area.

EL-PF-3.5

Consult with the Northshore Fire Protection District and Lake County Fire Protection District when reviewing new development proposals to identify any facility, infrastructure, or equipment upgrades needed to support the development and ensure the development pays for any identified upgrades.

EL-PF-3.6

Support development of local firewise communities and fire safe councils and work collaboratively to manage hazardous vegetation on private properties.

EL-PF-3.7

Promote environmental stewardship employment opportunities for local and Tribal communities that support forest health, prescribed burns, defensible space, watershed restoration, trail stewardship, ecological monitoring, native plant restoration, and cultural interpretation activities.

~~EL-PF-3.7~~EL-PF-3.8 *

Require that new development demonstrates adequate water supply and service for effective fire protection and that all infrastructure meets the fire protection standards of Lake County or the responsible agency.

~~EL-PF-3.8~~EL-PF-3.9

Promote public safety programs and activities, including neighborhood watch programs,~~child identification kits~~, fire hazard prevention and awareness, and other public education efforts.

Goal EL-PF-4

Affordable, reliable, and safe water and wastewater service that meets existing and future community needs.

Policies

EL-PF-4.1

Discourage extension of water and sanitary sewer lines into areas outside of Community Growth Boundaries, except to serve public and semi-public uses that are not growth inducing, or when such extension is necessary to address a declared public health emergency.

The related issue of water supply is addressed in the countywide Water Resources Element.

EL-PF-4.2

~~Encourage and support efforts to extend sewer service to existing communities.~~

EL-PF-4.3

~~Support efforts to upgrade Clear Lake Oaks and Spring Valley water and wastewater systems.~~

~~EL-PF-4.2~~EL-PF-4.4

Coordinate with ~~Lake County~~ LAFCO in the preliminary stages of development proposal entitlement application reviews for projects that may require an expansion of current service district boundaries to determine whether LAFCO will have a role in the approval process.

~~EL-PF-4.3~~EL-PF-4.5*

Require new development to demonstrate the availability of a safe, sanitary, and environmentally sound water delivery system with adequate capacity, including by securing a will-serve letter from the water purveyor within the service area prior to the issuance of construction permits, to ensure that the water purveyor has the capacity to supply water to the project. Have water purveyor delineate if interior fire sprinklers can be connected to the system.

EL-PF-4.4

~~Prioritize water conservation measures over the development of new water sources like dams and reservoirs.~~

EL-PF-4.5

~~Discourage the formation of new water distribution systems unless long-term fiscal and technical feasibility can be demonstrated prior to approval~~

EL-PF-4.6

Encourage efforts to consolidate small water distribution systems to improve service and financial stability, in alignment with the County's Drought Resilience Program.

EL-PF-4.7

Partner with water service providers to ensure that low-income households maintain continuity of service and are offered financial relief if prices rise.

EL-PF-4.8

Promote the development of and connection to municipal wastewater systems where failing septic tanks, leach fields, and package systems constitute a threat to water quality and public health that cannot be remedied otherwise.

EL-PF-4.9

Prioritize areas identified as having a high water table for connection to municipal wastewater systems.

EL-PF-4.10

Require new development subject to discretionary review and in areas identified as having a high water table to connect to municipal wastewater systems as a condition of approval.

EL-PF-4.11

Encourage the formation of septic maintenance districts as an interim management practice if on-site septic systems are documented to have contributed to violations of water quality standards.

EL-PF-4.12

Promote and support programs to educate homeowners on the care and maintenance of septic systems.

EL-PF-4.13

Prior to the approval of a building permit with a proposed addition/modification to an existing structure that would impose an additional burden on an existing system by increasing the waste flow, the buyer or project applicant shall demonstrate to the Lake County Environmental Health Department that the onsite wastewater treatment system is operating properly.

Actions**EL-PF-4.a**

Pursue all available State and federal funding sources to upgrade publicly managed community water systems.

EL-PF-4.b

Work with water service providers to upgrade substandard water systems to ensure adequate fire flows and backup power sources.

EL-PF-4.c

Update the County Code of Ordinances to comply with water quality regulations.

EL-PF-4.cEL-PF-4.d *

Periodically review standards for septic tanks to ensure they continue to protect water quality and public health, consistent with current best practices.

EL-PF-4.dEL-PF-4.e

Explore rural wastewater systems that can serve as alternatives to conventional systems and develop standards and guidelines for their use in new development, as appropriate.

EL-PF-4.eEL-PF-4.f

Develop a program to periodically test waterways, groundwater, and/or select private wells, as applicable, for contamination in areas where on-site septic system densities are greater than one dwelling unit per acre.

EL-PF-4.fEL-PF-4.g

Identify and map areas in the Spring Valley community with a high water table.

EL-PF-4.gEL-PF-4.h

Conduct water pollution investigations in areas suspected of having a significant number of failing on-site sewage disposal systems.

EL-PF-4.hEL-PF-4.i *

In water districts where there is insufficient water to serve new development, require new development to offset demand so that there is no net increase in demand through measures such as the installation of low-flow water fixtures, water conservation efforts, or the retrofit of existing uses to offset increased demand. These measures shall be achieved in partnership with the applicable water district.

EL-PF-4.iEL-PF-4.j *

Require For all properties, encourage renovations requiring a building permit to install low-flow water fixtures to reduce the amount of water usage, and require such fixtures for properties served by municipal water providers.

EL-PF-4.jEL-PF-4.k *

Require new development projects to obtain a will-serve letter from the water and wastewater agencyies within the project's service area prior to project approval to ensure that the wastewater agency has the capacity to accept wastewater from the project and that the water agency has capacity to provide water for interior sprinkler systems.

Goal EL-PF-5

Stormwater facilities that protect people, property, and the environment.

Policies

EL-PF-5.1

Ensure stormwater collection and drainage facilities are properly designed and sited to protect people and property while conserving water resources, supporting groundwater recharge, protecting water quality, and minimizing impacts on natural and cultural resources.

The countywide Water Resources Element contains additional strategies to address runoff, supporting both stormwater management groundwater recharge. The issue of flooding is addressed in the countywide Health and Safety Element.

EL-PF-5.2

Require curbs and gutters for new development in Community Growth Boundaries to direct storm drainage in a way that is consistent with the overall drainage system.

EL-PF-5.3

Require development projects to use green infrastructure and low-impact development designs to prevent off-site drainage impacts and support groundwater recharge.

EL-PF-5.4

Require on-site stormwater detention facilities and velocity reducers when necessary to maintain existing storm flows and velocities in natural drainage systems.

EL-PF-5.5

Encourage project designs that minimize drainage concentrations and impervious coverage, complement groundwater recharge, and where feasible, provide a natural watercourse appearance and maintain natural watershed functions.

EL-PF-5.6

Integrate green infrastructure, such as permeable paving surfaces, bioswales, and rain gardens, into County infrastructure projects, including road improvements and parks.

EL-PF-5.7

Encourage designs for stormwater detention basins that provide temporary or permanent wildlife habitat and/or recreational opportunities, where appropriate.

EL-PF-5.8

Reduce the flow of stormwater drainage into sewer cleanouts and other similar mechanisms that have the ability to inundate the local sewer collection system, including by enforcing penalties for violators purposely directing stormwater into sewer cleanouts and educating the community about how stormwater drainage can impact the local sewer collection system.

EL-PF-5.9

Encourage green infrastructure, Tribal Ecological Knowledge practices, wildlife habitat integration, and protection of culturally sensitive areas.

Actions**EL-PF-5.a**

Identify damaged and/or deficient storm drainage facilities and acquire funding to restore or upgrade the facilities, prioritizing facilities in areas subject to lake and localized flooding.

EL-PF-5.b

Explore development of an Adopt-a-Drain program, which may take inspiration from the program administered by the City of Oakland.

EL-PF-5.c

Work with appropriate agencies to develop stormwater retention/detention and recharge facilities to enhance flood protection and improve groundwater recharge.

EL-PF-5.d

Work with appropriate agencies to develop stormwater facility maintenance programs establishing timelines for regularly cleaning and maintaining storm drain systems.

Goal EL-PF-6

Accessible, affordable, safe, and efficient disposal or recycling of solid waste.

Policies**EL-PF-6.1**

Ensure efficient and affordable solid waste disposal options are available for all, including unhoused populations, low-income households, and other Equity Priority Populations.

EL-PF-6.2 *

Prior to project approval, require development proposals to demonstrate capacity within the solid waste system for the processing, recycling, transmission, and disposal of solid waste, and ensure that the development provides adequate solid waste storage, handling, and collection accommodations.

EL-PF-6.3

Encourage and support establishment of adequate sites for collection of household hazardous waste, unused pharmaceuticals, and universal wastes (commonly produced hazardous wastes, such as batteries and electronics), along with provisions for residents who are physically unable to deliver materials to a collection site.

EL-PF-6.4

Protect solid waste facility sites from the encroachment of sensitive and/or incompatible land uses and convert abandoned solid waste facilities to open space use.

EL-PF-6.5

Support efforts to establish and maintain recycling redemption centers.

EL-PF-6.5EL-PF-6.6

Promote the maximum feasible solid waste reduction and diversion through recycling and composting. Strive to continually reduce commercial and industrial waste on an annual basis and pursue a financing mechanism for solid waste reduction programs to meet reduction and diversion goals.

EL-PF-6.6EL-PF-6.7

Use recycled materials and products when available and economically feasible.

EL-PF-6.7EL-PF-6.8

Enforce prohibitions and fines against illegal dumping, abandoned property and vehicles, and outdoor storage, and prioritize clean-up of illegal dumping on County-owned land in community centers and in Equity Priority Areas.

EL-PF-6.8EL-PF-6.9

Support cleanup efforts by private property owners to remove garbage and refuse from their properties and obtain grant funding to assist in removing the refuse at no-cost waste disposal events.

Actions

EL-PF-6.a

Seek funding to support low- or no-cost waste disposal programs for unhoused populations, low-income households, and other Equity Priority Populations.

EL-PF-6.b

Promote community education about responsible waste disposal, including by distributing informational resources promoting alternatives to sending waste to landfills or burning green waste, such as recycling, composting, and chipping, and by supporting educational programs to teach youth about composting.

EL-PF-6.c

Investigate options to establish small transfer stations in each community.

EL-PF-6.cEL-PF-6.d

Continue to support community chipping and portable chipping programs.

EL-PF-6.dEL-PF-6.e

Install cameras and signage regarding the prohibition of dumping at locations with recurring issues and increase education, monitoring, and rapid cleanup to discourage future illegal dumping.

EL-PF-6.eEL-PF-6.f

Partner with community organizations to host community clean-up days, as needed, prioritizing events in downtown areas and Equity Priority Areas.

EL-PF-6.f**EL-PF-6.g**

Continue existing abatement programs that assist private property owners with removal of unsightly trailers, signage, and trash, prioritizing clean-up in Equity Priority Areas.

EL-PF-6.g**EL-PF-6.h**

Seek additional funding for the ongoing operation of the Abandoned Vehicle Abatement (AVA) program.

Goal EL-PF-7

Internet, cellular, and landline communications services that are accessible and reliable throughout the East Lake Communities Planning Area.

Policies

EL-PF-7.1

Encourage and support efforts to expand and enhance internet and broadband service throughout the East Lake Communities Planning Area and encourage co-location of infrastructure and marketplace competition from multiple service providers, while minimizing visual and cultural impacts.

EL-PF-7.2

Encourage and support efforts to improve cellular phone coverage and reliability, especially during extreme weather and hazard events.

EL-PF-7.3

Require new or updated cellular towers to be designed to minimize their visual impact.

Actions

EL-PF-7.a

Work with the California State Association of Counties and others to lobby the State to require continued access to landline telephones.

EL-PF-7.b

~~Identify and implement strategies to streamline the permitting process for cellular tower installations while ensuring quality design and mitigation of potential impacts.~~

Goal EL-PF-8

High-quality schools and educational facilities that serve the needs of local students.

Policies

EL-PF-8.1

Collaborate with the Konocti Unified School District, Lucerne Elementary School District, and Upper Lake Unified School District to plan for future growth.

EL-PF-8.2

Work with the Konocti Unified School District, Lucerne Elementary School District, and Upper Lake Unified School District to ensure any new schools that are developed in the Planning Area incorporate bicycle and pedestrian access, are close to the residential areas that the school serves and avoid hazards.

EL-PF-8.3

Support efforts to retrofit and/or upgrade existing educational facilities to ensure safety and comfort of students and educators, including through expedited permitting reviews and fee waivers, where feasible.

EL-PF-8.4

Encourage and support efforts to enhance and expand access to higher education, including by working with Woodland Community College Lake Center and Mendocino College Lake Center to increase access to transferable general education and major preparation courses in alignment with the University of California and California State University transfer pathways and articulation agreements.

EL-PF-8.5

Encourage and support efforts to incorporate tribal cultural education and awareness in school curriculums.

Action**EL-PF-8.a**

Coordinate residential development proposal reviews with the Konocti Unified School District, Lucerne Elementary School District, and Upper Lake Unified School District to inform future enrollment estimates and ensure adequate capacity for the development project.

Economic Development

A healthy local economy is an important aspect of community life. It can affect whether there are jobs that pay living wages available in the community, in turn affecting household incomes. Successful local businesses and families earning living wages affect how well they maintain their properties and their ability to contribute to local taxes that sustain public facilities and services. A healthy local economy can also support small-scale commercial development that attracts visitors and brings the community together.



Thoughtfully planned new development can support the local economy while preserving local character.

This element works to promote economic development in the East Lake Communities Planning Area, emphasizing support for locally owned, small businesses and restaurants; expanding tourist activities, entertainment options, and events; enhancing the Planning Area's identity as a gateway to Mendocino National Forest, and increasing high-paying job offerings.

Local Economy

The Planning Area's commercial activities primarily occur along the SR 20 corridor. As discussed in the Introduction, development in the Planning Area was catalyzed in the late 1800s by the construction of resorts, which capitalized on their proximity to Clear Lake and other unique natural features like mineral springs. Bartlett Springs Resort, Hough Springs Resort, and others were once crucial contributors to the economy of the East Lake Communities Planning Area. Today, few of these establishments remain due to economic downturns, destruction from wildfire, and abandonment. Clear Lake, its shores, and other natural features are the Planning Area's greatest economic asset, particularly for tourism and recreation purposes. Residents have expressed interest in the rehabilitation of historic resorts and revitalization of tourism activity along the shores of Clear Lake that was common in the early and mid-twentieth century.

Since its approval in 2011, the Lake County Economic Development Strategic Marketing Plan has helped increase visitation, attract tourism investment, and increase the economic impact of the tourism industry countywide, including in the East Lake Communities Planning Area. Local tourism industry stakeholders, including lodging establishments, restaurants, caterers, retailers, wineries and tasting rooms, events and attractions, tribal gaming, event venues, and local business associations, collaborated with a Strategic Advisory Group to prepare the plan.

In addition to Clear Lake, which offers opportunities for fishing, boating, watersports, wildlife viewing, and other recreational activities, the Planning area hosts a number of vineyards and wineries including Brassfield Estate Winery, Cache Creek Vineyards and Winery, and Stonehouse Cellars. Mendocino

National Forest is another major destination with a variety of recreational options: biking, hiking, horseback riding, camping, hunting, fishing, off-highway vehicle riding, picnicking, and more.

There are several organizations dedicated to supporting the local economy of the East Lake Communities Planning Area. The Clear Lake Chamber of Commerce, based in the City of Clearlake, organizes events, provides business resources, and advocates on behalf of local businesses. The Clearlake Oaks – Glenhaven Business Association promotes community improvement and commerce by supporting local businesses, serving the communities of Clearlake Oaks and Glenhaven.

Employment

According to the Longitudinal Employer Household Dynamics (LEHD) survey administered by the U.S. Census, there were almost 4,500 employed residents in the East Lake Communities Planning Area as of 2022, the latest year for which LEHD data is available. As detailed in the Introduction, major employers in the Planning Area span several industries, including hospitals, clinics, skilled nursing care, retail, and junior and community college institutions. Data provided by the State of California’s Employment Development Department Labor Market Information Division shows that the unemployment rate was 10.9 percent in the City of Clearlake and 10.6 percent in the Clearlake Oaks community as of March 2025.

Based on the 2022 LEHD survey, roughly 22 percent of residents in the East Lake Communities Planning Area work in the City of Clearlake, and 3 percent work in the community of Clearlake Oaks. An additional 12 percent work elsewhere in Lake County, and the remaining 63 percent of residents in the Planning Area commute to other cities in Sonoma, Napa, Mendocino, Marin, or Solano Counties,, primarily to Santa Rosa in Sonoma County (4 percent) or to Ukiah (2 percent) in Mendocino County.

Goals, Policies, and Actions

Goal EL-ED-1

A strong, diverse, and resilient economy that supports local employment opportunities.

Policies

EL-ED-1.1

Continue to develop a diversified economic base for the East Lake Communities Planning Area by promoting the Planning Area as the gateway to Clear Lake recreational activities, the Mendocino National Forest, and local commerce and by expanding efforts to encourage industrial and non-industrial corporate uses and geothermal resource development.

EL-ED-1.2

Support economic development strategies that promote local procurement, strengthen business resilience to power shutoffs, encourage the adaptive reuse and site improvements of existing buildings, and expand environmental stewardship employment opportunities.

EL-ED-1.3

Establish incentives that encourage local ownership, investment, and development by Lake County residents.

EL-ED-1.4

Encourage and support local education and job opportunities for careers in health care and law enforcement.

EL-ED-1.2EL-ED-1.5

Prioritize business retention, expansion, and attraction efforts that strengthen the local economy, generate local tax revenue, and/or offer jobs in alignment with local skills.

EL-ED-1.3EL-ED-1.6

Purchase products and services for County operations from locally owned businesses whenever possible.

EL-ED-1.7

Work with local stakeholders to define a “creative economy” and develop strategies to cultivate a creative economy through support for artist studios, galleries, and live/work spaces.

EL-ED-1.4EL-ED-1.8

Consult with local Tribal governments in the design of economic recovery and job creation strategies to ensure culturally appropriate, place-based employment opportunities.

EL-ED-1.5EL-ED-1.9

Acknowledge local tribal communities as an economic stakeholder and encourage opportunities for tribal enterprises that align with cultural preservation, environmental stewardship, and community character.

EL-ED-1.6EL-ED-1.10

Support workforce-development partnerships benefiting tribal citizens and incorporating Tribal Ecological Knowledge, protecting Tribal Cultural Resources, and/or promoting sustainable land stewardship, where appropriate.

EL-ED-1.11

Support broadband-enabled economic development activities, including remote work, telework hubs, coworking space, digital entrepreneurship, and online business support.

Actions**EL-ED-1.a**

Coordinate efforts with local business associations and stakeholders to attract clean industries to the area. Ensure new clean industries are located and designed to avoid Tribal Cultural Resources.

EL-ED-1.b

Partner with private and non-profit agencies to provide business incubation and workforce training to help economic vitality and foster employment, including tribal workforce partnerships and training for tribal citizens in cultural resource protection, environmental restoration, and sustainable economic practices.

EL-ED-1.c

Establish incentives that encourage local ownership, investment, and development by Lake County residents.

~~EL-ED-1.c~~EL-ED-1.d

Identify or develop programs to expand economic opportunities to Equity Priority Populations, including through outreach efforts and incentives for local small business owners and tribal citizens and enterprises.

~~EL-ED-1.d~~EL-ED-1.e

Explore options to offer development impact fee discounts or exemptions or other incentives when applicants can demonstrate economic benefits for the community.

~~EL-ED-1.e~~EL-ED-1.f

Work with stakeholders in the area to explore development of a transitional employment program that integrates unhoused individuals into public or private sector work focused on neighborhood beautification and maintenance, prioritizing central commercial areas and other locations.

~~EL-ED-1.f~~EL-ED-1.g

Investigate the benefits of establishing a business license program.

~~EL-ED-1.g~~EL-ED-1.h

Expand County support for business growth and development, including by partnering with outside agencies and associations working in economic development.

~~EL-ED-1.h~~EL-ED-1.i

Identify and eliminate barriers to supporting home-based business as part of the local, circular economy.

~~EL-ED-1.i~~EL-ED-1.j

Develop strategies to encourage shopping at locally owned businesses whenever possible.

EL-ED-1.k

Investigate the benefits of establishing a cannabis retail license for oversight.

Goal EL-ED-2

A strong and resilient tourism sector that contributes to the local economy while balancing community and visitor needs and priorities.

Policies

EL-ED-2.1

Reestablish the East Lake Communities Planning Area as a premier vacation destination by growing local attractions, including resorts, hotels, entertainment venues, regional events, wineries, and visitor activities.

EL-ED-2.2

Encourage and support a small-business and adaptive-reuse assistance program for older resorts, retreat centers, and underused commercial properties.

EL-ED-2.2EL-ED-2.3

Encourage and support development of wineries, microbreweries, distilleries, farm stands, U-pick operations, and other agritourism related uses, provided those uses do not negatively impact ongoing agricultural operations and they avoid disturbance of known or potential Tribal Cultural Resources.

EL-ED-2.3EL-ED-2.4

Support the creation and expansion of commercial facilities and events that attract tourists to the area.

EL-ED-2.5

~~EL-ED-2.4~~ Encourage restoration, rather than replacement, of unique and historic resorts and related facilities, when practical.

EL-ED-2.5EL-ED-2.6

Consider approval of proposals for mixed-use resorts requesting increased residential density outside of Community Growth Boundaries when such densities are needed to make the project economically feasible and the resort will provide important public benefits, including advancing the County's tourism objectives and providing recreational facilities to the public.

EL-ED-2.6EL-ED-2.7

Continue to improve visitor services in the East Lake Communities Planning Area.

EL-ED-2.7EL-ED-2.8

Encourage and support efforts to improve water quality in Clear Lake to boost the tourism economy and improve quality of life for residents.

EL-ED-2.8EL-ED-2.9

Encourage tourism initiatives that interpret, respect, and help protect tribal cultural heritage.

EL-ED-2.9EL-ED-2.10

Support the development of tribal-led or tribal-partnered cultural education programs, guided tours, and visitor materials—where appropriate and approved by the tribes—ensuring protection of sensitive locations.

Actions

EL-ED-2.a

Partner with other interested parties to leverage and expand promotional efforts to attract visitors, especially multiday visitors, to the county's diverse attractions, including Mendocino National Forest, agri-tourism, ecotourism, outdoor recreation, and various lodging options. Require all promotional materials referencing tribal history or cultural sites to be reviewed by the affiliated tribes to ensure accuracy and to avoid disclosing sensitive locations.

EL-ED-2.b

Update the Zoning Ordinance to allow farm-stay development, which would allow volunteers to stay at farms with compensation in exchange for their work on the farm as an apprenticeship, on appropriate agricultural sites.

EL-ED-2.c

Partner with local groups and agencies to develop and distribute resources for tourists to identify and access the many opportunities to experience Lake County, such as self-guided tour maps, farm trail maps, and/or informational kiosk installations. Exclude sensitive Tribal Cultural Resources from public maps and develop tribal-approved interpretive content, where appropriate.

EL-ED-2.d

Identify incentives and other opportunities to encourage and support development, redevelopment, and expansion of lodging spaces to serve a broad range of visitors, including luxury boutique hotels and resorts, retreat venues, conference centers, family oriented motels, hot springs, and camping areas. Ensure that any new or expanded uses are compatible with the surrounding environment, [wastewater capacity, and account for emergency ingress/egress and emergency needs.](#)

EL-ED-2.e

Amend the Zoning Code to establish requirements for mixed-use resorts that request increased density outside of Community Growth Boundaries. Requirements should ensure the following:

1. The project will advance the County's tourism objectives.
2. The resort provides substantial resort and recreational facilities that will be available to the public.
3. The developer demonstrates that the additional residential units are necessary to support the infrastructure and public resort amenity costs for the overall project and the overall project is economically infeasible without the additional residential units.
4. The primary scope of the project is resort commercial.
5. The residential component is secondary and subordinate.

-
6. There are fewer residential units than resort units, unless the project is adjacent to a Community Growth Boundary and public water, wastewater, and school services are available to serve the development, in which case the ratio of residential units to resort units must not exceed 2:1.
 7. Residential densities are less than one residential unit per gross acre.
 8. Applications are submitted as Planned Developments to determine the appropriate number of residential units allowed.

EL-ED-2.f

Explore options to dedicate a portion of the County's Transient Occupancy Tax revenue to efforts to conserve natural resources to maintain and enhance the county's natural beauty.

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Special Study Areas and Design Guidelines

There are two Special Study Areas (SSAs) in the East Lake Communities Planning Area – “The Plaza” Clearlake Oaks and East Clearlake Oaks Commercial District. These SSAs are places where the community has a more detailed vision for future development; to support the community’s vision, this General Plan provides customized policy guidance for those areas. This section details the visions and supporting goals, policies, and actions, for each SSA, in addition to design guidelines applicable to all of Clearlake Oaks, including both SSAs.

The design guidelines serve to ensure new development and redevelopment complement the existing built and natural environments and foster attractive buildings and landscaping that maintain the community’s uniqueness, enhance its visual character, and support business attraction efforts. Design guidelines are used by County staff and the Planning Commission in evaluating changes to existing properties and new construction.

“The Plaza” Special Study Area

Note: “The Plaza” Special Study Area was originally prepared in 2009 and will be updated through a community-driven process through the implementation of this General Plan, in accordance with Action EL-LU-3.c.

“The Plaza” SSA includes all properties on Foothill Boulevard between Pine Street to the south and Oak and Acorn Streets to the north and those properties along the Plaza until its junction with Pine and Oak Streets to the west, in addition to all property adjacent to Short Street, as shown in **Figure EL-17**.

Development of the “The Plaza” as a community gathering place and town center was identified by community members developing the Northshore Redevelopment Plan as a top priority for Clearlake Oaks. This SSA also includes development across the highway along Short Street. Short Street is a horseshoe-shaped road that connects to SR 20 at both ends and is bordered on the south side by Stubbs Canal. This is a unique locale in the Planning Area as it lies within walking distance to the town center and has access to Clear Lake. Development at this site will need to address issues such as pedestrian access across SR 20, flooding, and existing residential uses present in the immediate vicinity. This SSA is designed to implement the basic land use foundation for the flexible and incremental development of properties surrounding “The Plaza.”

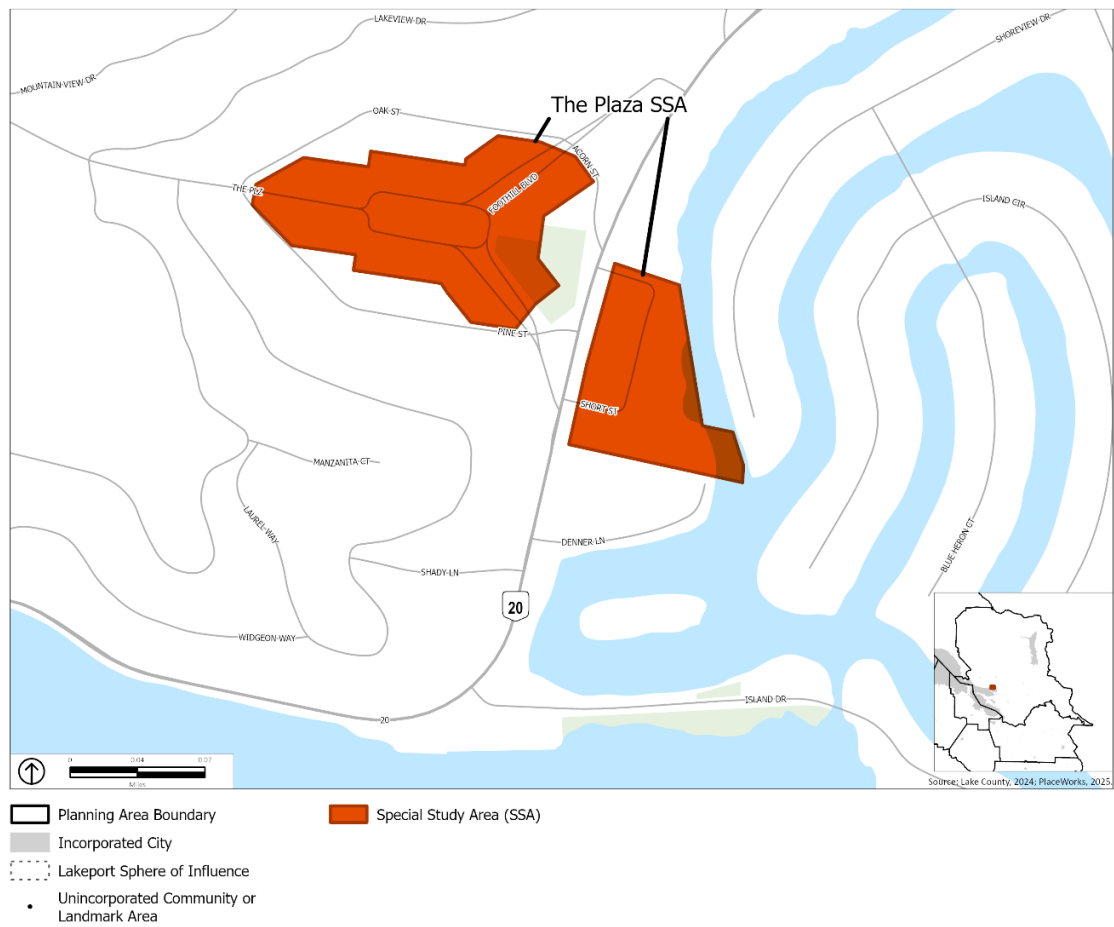
Desired uses in the SSA include retail, dining, professional and personal services, banks, limited lodging, and other similar uses, in addition to residential uses above and behind shops. Uses along Short Street should incorporate the benefits of having waterfront access and build on existing uses in the nearby locale. Other potential options include the construction of a boardwalk along Stubbs canal and the development of additional lodging, dining, and boutique retail.

Development of “The Plaza” and surrounding vicinity should focus on creating a viable town center and community gathering space with a mix of retail, dining, essential services, and some residential above and behind shop fronts. Streetscape and building design and location should promote a pedestrian friendly atmosphere through the promotion of “complete streets” and “walkable community” principles. A comprehensive parking plan should be developed for the entire project to encourage people to leave their vehicles and travel storefronts and the Plaza on foot. Parking areas should be tastefully landscaped and maintained to complement the town center atmosphere without detracting from nearby residences. A comprehensive landscaping plan for the Plaza should also be considered that includes street trees, planter areas, sidewalks, lighting, and frequent benches.

Lots in the Plaza are small and narrow. Building design should be concentrated on the maximum use of space. This should include the location of buildings near sidewalks and side property boundaries. Where necessary, lots should be merged to facilitate the best use of available space. Two-story buildings should be encouraged. The creation of a comprehensive parking plan for the entire town center is essential to achieving these goals.

Development of the Short Street area should be done in conjunction with traffic calming and pedestrian improvements to SR 20. Commercial uses along Short Street should be pedestrian oriented, thus a comprehensive parking plan for the whole area would need to be completed to ensure the maximum use of available storefront area. This would also advance the aesthetic quality of the downtown area, making for attractive views of Clear Lake and increasing the likelihood that passing motorists will stop in Clearlake Oaks, improving the total economic viability of this area.

Figure EL-17 “The Plaza” Special Study Area



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Goals, Policies, and Actions

Goal EL-SSA-3

A concentrated, vibrant town center in Clearlake Oaks that, in conjunction with “The Plaza,” provides services to local residents and tourists.

Policies

EL-SSA-3.1

Provide a framework for a mix of commercial activities aimed at serving both tourists and local residents.

EL-SSA-3.2

Promote retail, dining, professional and personal services, banks, and other similar uses. Projects should be designed to include features that create a pedestrian mall linking development with “The Plaza” and community gathering spaces.

Actions

EL-SSA-3.a

Develop a comprehensive parking plan for “The Plaza” and other areas in the downtown Clearlake Oaks business district.

EL-SSA-3.b

Develop a special fund paid into by new commercial development for the acquisition and establishment of public parking. Incorporate the use of the “P” Parking Combining District. Encourage the development of public parking lots throughout the vicinity to better facilitate pedestrian use and maximize the usability of parcels for commercial use.

EL-SSA-3.c

Devise a set of sample commercial building plans and elevations for prospective developers in “The Plaza” that incorporate desired design characteristics and address common building constraints, including the need to maximize usable area.

Goal EL-SSA-4

A small-scale retail hub that complements “The Plaza” focused on serving a pedestrian clientele and using the benefits of water frontage.

Policies

EL-SSA-4.1

Promote an environment conducive to small-scale retail, dining, and resort uses along Short Street.

EL-SSA-4.2

Promote a pedestrian-friendly environment along Short Street through appropriate streetscape and building design.

EL-SSA-4.3

Require projects to be designed to include pedestrian features, such as boardwalks and sidewalks.

EL-SSA-4.4

Consider development of public parking areas on properties not suitable for commercial development, such as areas prone to flooding.

EL-SSA-4.5

Strictly enforce 30-day stay limits for hotel and resort uses in the vicinity.

Actions**EL-SSA-4.a**

Incorporate Short Street into a comprehensive parking district that includes “The Plaza” and downtown community district.

EL-SSA-4.b

Install directional signage to public parking areas in the vicinity.

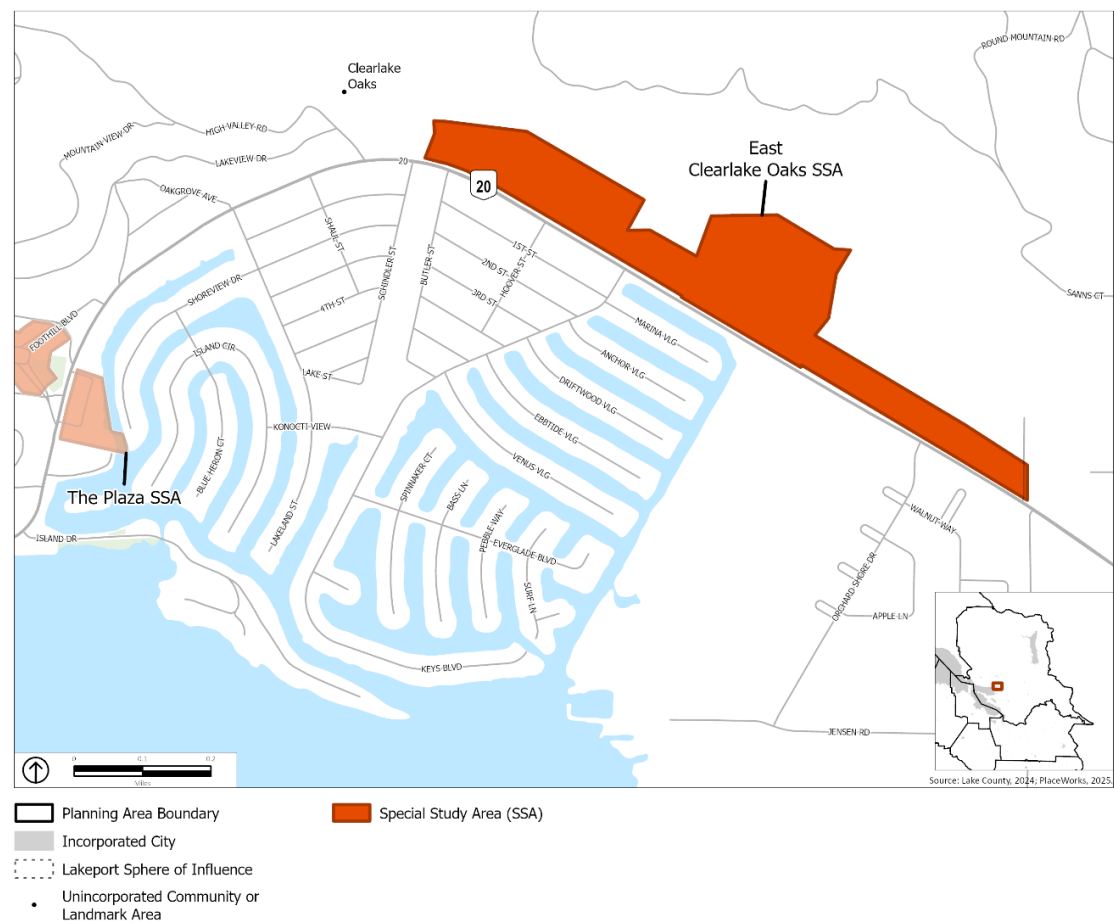
East Clearlake Oaks Commercial District Special Study Area

Note: The East Clearlake Oaks Commercial Special Study Area was originally prepared in 2009 and will be updated through a community-driven process through the implementation of this General Plan, in accordance with Action EL-LU-3.c.

The East Clearlake Oaks Commercial District SSA consists of the area north of SR 20 between East Lake Elementary School to the northwest and Round Mountain Road to the southeast, as depicted in **Figure EL-18**.

Commercial uses in Clearlake Oaks are currently split between the area near “The Plaza” and the East Clearlake Oaks Commercial District. Critical to the development of a town center at “The Plaza” is the creation of an adequate land use strategy that will ensure that other commercial development in the community will not detract from the overall viability of “The Plaza.” Instead, the development in the East Clearlake Oaks Commercial District should complement the town center by providing the types of commercial services not well suited to the town center. The East Clearlake Oaks Commercial District also acts as the visual gateway to the community of Clearlake Oaks and Lake County from SR 20.

Figure EL-18 East Clearlake Oaks Commercial District Special Study Area



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Future development in the East Clearlake Oaks Commercial District should concentrate on providing commercial retail-oriented businesses along the highway frontage, allowing for separation from more intensive commercial service-oriented uses at the rear of this site. Medical offices, professional commercial services, and other services not well suited for “The Plaza” should be encouraged. Uses such as mini-storage, contractor’s yards, and other similar uses should be discouraged along the highway frontage and be behind other lower-intensity uses. A wide variety of uses should be encouraged in this area.

The screening of parking areas through tasteful landscaping and street tree plantings to create a visual break between the highway and commercial uses should be promoted. Comprehensive sign programs should be developed for this entire area aimed at concentrating on-site advertising to building frontages and marquee signs containing several businesses along SR 20. Large individual advertising signs along the highway frontage should be avoided.

Goals, Policies, and Actions

Goal EL-SSA-5

Focused commercial development on the north side of SR 20 at the eastern end of Clearlake Oaks.

Policies

EL-SSA-5.1

Use streetscape and landscape features between SR 20 and commercial parking areas to provide a visual break. Require that new development screen parking areas from views from the highway and commercial uses through appropriate landscaping.

EL-SSA-5.2

Avoid large individual advertising signs along the highway frontage and prohibit off-site billboard type advertising.

Action

EL-SSA-5.a

Develop a comprehensive sign program for the East Clearlake Oaks Commercial District aimed at concentrating on-site advertising to building frontages and marquee signs containing several businesses along SR 20. As part of this sign program, prohibit off-site outdoor advertising signs.

Clearlake Oaks Community Area Design Guidelines

Note: The Clearlake Oaks Community Area Design Guidelines were originally prepared in 2009 and will be updated through a community-driven process through the implementation of this General Plan, in accordance with Action EL-LU-3.d.

The growth of commercial services in the community center of Clearlake Oaks is expected to continue over time. Most of the development in the town's center will take the form of infill on vacant property or replacement of existing structures. The following design guidelines are a series of recommendations that are aimed at helping Clearlake Oaks establish a commercial design style befitting this rural waterfront community, with consideration for new infill, renovation, and replacement building type projects.

Building Setback

Setback distance is measured from the property line to the building. Side setbacks may also need to be considered where there is a uniform pattern near the location of a new building.

Observations

- Clearlake Oaks has typical setbacks of 10 to 20 feet from the road edge.

Guidelines

- In locations where the setback is readily discernible and uniform, new infill construction should respect and follow this pattern of development.
- In locations where setbacks are more distant from the roadway, but have a perceivable degree of uniformity, new buildings should also use the same setbacks.
- In locations where there is not any uniformity, setbacks for infill development should be considered as to how they will visually impact surrounding uses. Location of a new building directly adjacent to a road edge, where neighboring buildings are set further back, is not necessarily adverse – it depends on the new building's use, scale, width, and height.

Building Height

With few exceptions, each community is predominated by one-story structures.

Observations

- Clearlake Oaks' core has a limited number of two-story buildings.
- The remainder of buildings in Clearlake Oaks are one-story.

Guidelines

- New buildings should respect the predominant height of the surrounding buildings – they should match (or nearly so) the height of neighboring, adjacent structures, where the new building has a similar setback to its road, or curblane, compared to its neighbors.
- Taller structures should not be discouraged, but a greater setback of the taller portion of the structure should be considered, where adjacent buildings are separated or recessed from the property line.
- Two-story buildings should be encouraged to create a strong presence in the town center.

Lot and Building Width

The width of a building lot facing the street is a very strong design feature. In Clearlake Oaks, lot width is relatively uniform.

Observations

- Clearlake Oaks has generally uniform lot widths along the lake side of the highway and a variety of widths along the other side of the highway.
- There is no perceptible rhythm of lot width, as some structures take up more than a single lot, and many lots are vacant.

Guidelines

- In locations where there is a strong repeating rhythm of lot width, created by uniformity, new construction should follow this width.
- Where new buildings take up two or more lots, adjacent to uniformly wide existing structures, the new building façades should be divided into a pattern that repeats the widths of its neighbors, if possible.
- Where new buildings are placed on empty lots between existing buildings that have differing widths, the new buildings should be rhythmically divided to repeat its predominant neighbor.

In addition to these design guidelines, new infill, façade improvements, and building replacement projects should continue to formulate projects that are compatible with design-oriented plans, such as the continued development of plans for “The Plaza” and Short Street, SR 20 Traffic Calming and Beautification Plans, and other related efforts. Together, these programs and guidelines will ensure the continued growth and viability of commercial businesses in the community of Clearlake Oaks.

Façade Improvements

Clearlake Oaks does not have a cohesive architectural theme linking buildings in a recognizable style. To create a cohesive downtown commercial district, new commercial development should follow a single architectural style. A community opinion survey conducted in 2007 indicated that most of the population of Clearlake Oaks wished to evoke a small town, rural feel to their community. New construction,

renovation of existing buildings, and infill development in the community reflect the rural charm of resort development along the Northshore in the first half of the twentieth century. Façade treatments need not be elaborate to provide a much more positive image for the community. The removal of poorly designed, out-of-scale signs, and repainting of the façade is often sufficient to accomplish a remarkable makeover.

Façade Materials

This guideline concerns the choice of materials that will predominate the façade. In many instances, the façade material becomes a strong character-defining element.

Observations

- Clearlake Oaks has stucco, wood, and brick façades, with no single material predominating.

Guidelines

- Where a material is predominant on one façade, the use of that same material for new adjacent buildings should be strongly encouraged.
- Real materials, of the best quality affordable, should be used. Fake materials (plastic rock, fake brick, etc.) should be strongly discouraged.
- Glazing should not be reflective, but clear or lightly tinted.
- Natural-stained wood cladding should be discouraged, since it does not exist to any significant degree. Painted wood should be used.
- Brick and stone can be used where there is not an otherwise predominant material in use.
- As a theme tying various buildings together in the community, the use of stone as a wainscot should be encouraged.

Horizontal Rhythms

This design element concerns the use of horizontal band on façades, such as cornices, awnings, or projecting bands that divide the first floor from the second. Usually, respect for these design elements should be encouraged. The exception is when horizontal features, such as a strong, heavy awning obscures the individuality of neighboring façades.

Observations

- Clearlake Oaks does not have any predominant horizontal façade elements that carry over from building to building, creating a theme or rhythm to the commercial district.
- Individual buildings in Clearlake Oaks have horizontal elements, which, if built next to, should be recognized in new construction.

Guidelines

- New buildings and façade remodels should respect the horizontal elements of neighboring buildings where those existing elements are original to the faces and not added on appendages.
- New infill buildings should include horizontal bands, such as string courses, parapet cornices, top friezes, and similar elements, that will also align with those elements on its immediately adjacent neighbors.

Windows and Doors

There are two design issues related to the openings in a building's façade. The first is the overall proportion, spacing, size, and shape of these openings. The second is the level of detail or division in the openings, particularly with regard to commercial storefront display windows.

Observations

- Clearlake Oaks' building façades contain a variety of opening sizes and proportions.
- Very few buildings contain windows or doors that have detail – that is, division of the glazed areas into smaller panes of glass, or division of doors with multiple solid or glazed panels, or by trim pieces.

Guidelines

- New one-story construction should be encouraged to have horizontal window openings, matching the predominant appearance of its neighbors.
- Rectangular openings should be encouraged. Round, triangular, or irregular openings should be discouraged, as they do not appear in any existing buildings.
- When appropriate to the style and era of existing buildings, window openings for retail façades should be divided into smaller panes of glass by mullions.
- False mullions should be discouraged. True divided-lite glazing is preferred, compared to snap-on interior mullions or thin mullion strips sandwiched between layers of glass.
- New buildings should be encouraged to provide window and door openings that maintain the predominant proportion and spacing of openings of existing buildings.

Color

Often the source of heated community debate, color guidelines are definable.

Observations

- Clearlake Oaks has a strong predominance of light-colored building façades and dark-colored roof edges.

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- A similar tonal pallet should be encouraged, although the use of warmer tones than now exists in Clearlake Oaks would be preferable.

Guidelines

- Bright primary colors, often associated with national chain stores, should be discouraged.
- Warm colors, which relate to the natural materials used on some buildings, should be encouraged.
- More intense, contrasting colors should be limited to details.
- Buildings should have colors that coordinate, not match, their neighbors.
- Color pallets on façades should be kept simple; two or three colors are usually sufficient to accent detail and provide variety.

Roof Shape

Commercial buildings tend to have horizontal roof lines. Sloping, hipped, or gable-end roofs are more common in residential structures. The shape of the roof, its parapet line, and its decorative details are all important in defining the characteristics of a district.

Observations

- Clearlake Oaks has a wide variety of roof forms, ranging from traditional commercial roofs (horizontal and stepped parapets), to sloping residential-style roofs, to mansard roofs.

Guidelines

- New buildings should have a roof form appropriate to their commercial nature. That is, sloping, exposed roofs, especially hip roofs facing the street, should be discouraged. An exception would be to create a gable or hipped roof for a portion of the building, where that portion signifies something important about its location, such as at an entry point to the building.
- Sloping parapets can be introduced in remodels or new construction to break up the monotony of only horizontal roof forms.
- Mansard roofs should be discouraged. This roof type is generally associated with national commercial chains and is not in keeping with the unique characteristics of these communities.

Awnings and Canopies

Horizontal, projecting awnings and canopies can be very effective design elements. They can also predominate a façade in a way that detracts from the façade's unique character. Awnings and canopies are a good location for signage, stenciled on the front edge of the awning.

Observations

- Clearlake Oaks has few awnings; however, roof overhangs are somewhat prevalent.
- The Red and White Market has a strong overhanging awning that should be preserved.

Guidelines

- Cloth awnings and canopies should be encouraged.
- Canopies can provide scale, color, and sign graphics in positive ways. Their overall appearance and impact on the building façade should be considered.
- Wood or metal awnings should not be encouraged, unless they truly appear integral to the design of the building façade and do not appear as a false appendage.

Signage

Signs can strengthen a building's appearance or easily detract from it.

Observations

Clearlake Oaks has generally poor signage. The signs are often too large, too high, and usually out-of-scale with the building.

Guidelines

- Abandoned signs should be removed as soon as possible.
- Plastic-faced, back-illuminated signs, which project perpendicularly from the building façade, should be strongly discouraged.
- Backlit signs with a light-colored field and dark lettering should be discouraged.
- Roof or parapet-mounted signs should be discouraged.
- Signs that obscure the roof line should be discouraged.
- Flush-mounted signs below the parapet are encouraged.
- Signs that appear as large as the building should be discouraged.
- Tall signs should be discouraged.
- Sign graphics on cloth canopies should be encouraged.
- Hanging signs, which are small and simple, should be encouraged.
- Window signs, which are small and simple, should be encouraged.
- Highly graphic or iconographic, symbolic signs (such as a donut for a bakery, or a book for a bookstore) should be strongly encouraged.
- Signs that are lit from an external source can be used.

Landscaping

The use of plant and hardscape materials in front of a building is an important design element.

Observations

- Some areas of Clearlake Oaks have an underdeveloped strip of paving or dirt in front the businesses.
- Individual property owners should enhance their public frontage with landscaping and paving materials.

Guidelines

- Decorative paving should be encouraged.
- Plant materials should be coordinated with a community tree planting program to complement, not compete, with community tree selections.
- Plant selections should be low maintenance, and not fragile to pedestrian usage.
- Non-native plants should be generally discouraged.
- Street furnishings (benches, light poles, trash receptacles, etc.), if not provided by individual property owners, should be coordinated with the efforts of other property owners to minimize a scattered, hodgepodge appearance.
- Fencing to block unsightly areas should be encouraged (typically in side yards).
- Fencing should match the building material wherever possible. If non-matching materials are used, fencing should have colors that blend with the predominant building material.